

# Downtown Ocoee Master Plan

City of Ocoee, Florida  
2016



# Acknowledgments

## **Prepared for the City of Ocoee, Florida**

### **City Council**

Mayor Rusty Johnson

District 1 Commissioner John Grogan

District 2 Commissioner Rosemary Wilsen

District 3 Commissioner Richard Firstner

District 4 Commissioner Joel Keller

### **City Manager**

Robert D. Frank

### **Development Services**

Craig Shadrix - Assistant Manager / Development Services Director

Michael Rumer - City Planner

**Prepared by GAI's Community Solutions Group - Tindale Oliver - Kittelson & Associates, Inc.**

# Downtown Ocoee Master Plan

City of Ocoee, Florida

December 2016



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D.J.'s AUTO SALES



Introduction

**in**







## PURPOSE OF THE PLAN

The Downtown Master Plan is the springboard to the rediscovering of downtown Ocoee and the rallying cry for the citizens and city leaders to fulfill the vision of a long-deserved rebirth of this gem by the lake.

The Ocoee Downtown Master Plan is the culmination of a six month public process to chart a new path forward for Ocoee's historic downtown, adjacent neighborhoods, and lakefront. The Plan is a frank introspection and acknowledgement of the current state of downtown and the factors contributing to it, a community-led Vision for transformation that builds on downtown's strengths, and a series of infrastructure investments,

policy changes, and regulatory updates that will help move downtown towards achieving the established Vision. This Plan comes at a time when public will, political leadership, and market forces have all built the momentum necessary for major positive changes to occur in the downtown. Capitalizing on this momentum in the short term and building on it in the long term will be critical to the success of the vitalization of downtown.

## COMPONENTS OF THE PLAN

### Study Area Analysis

This section is an evaluation of the current regional context, policy environment, built environment, and natural environment within which the downtown is set. All of these topics have driven the historical development of downtown and will continue to play a major role in how it develops in the future.

### Downtown Code Overlay

This is the regulatory implementation of the Master Plan Vision, describing the desired built form of the specific character areas of downtown, and helping to ensure that new development achieves the goals of the Vision process.

### Master Plan Vision

This section describes the community-driven Vision for the future development of downtown Ocoee. It establishes design frameworks for Connectivity, the Lakeshore, Activities, Character and Public Image, and Business Development, as well as specific implementation projects.



PERFORMANCE TROPHIES

PERFORMANCE TROPHIES  
AD SPECIALTIES & MODULAR SIGNS  
RIBBONS BADGES CUSTOM PINS

Mckey St Cumberland Ave

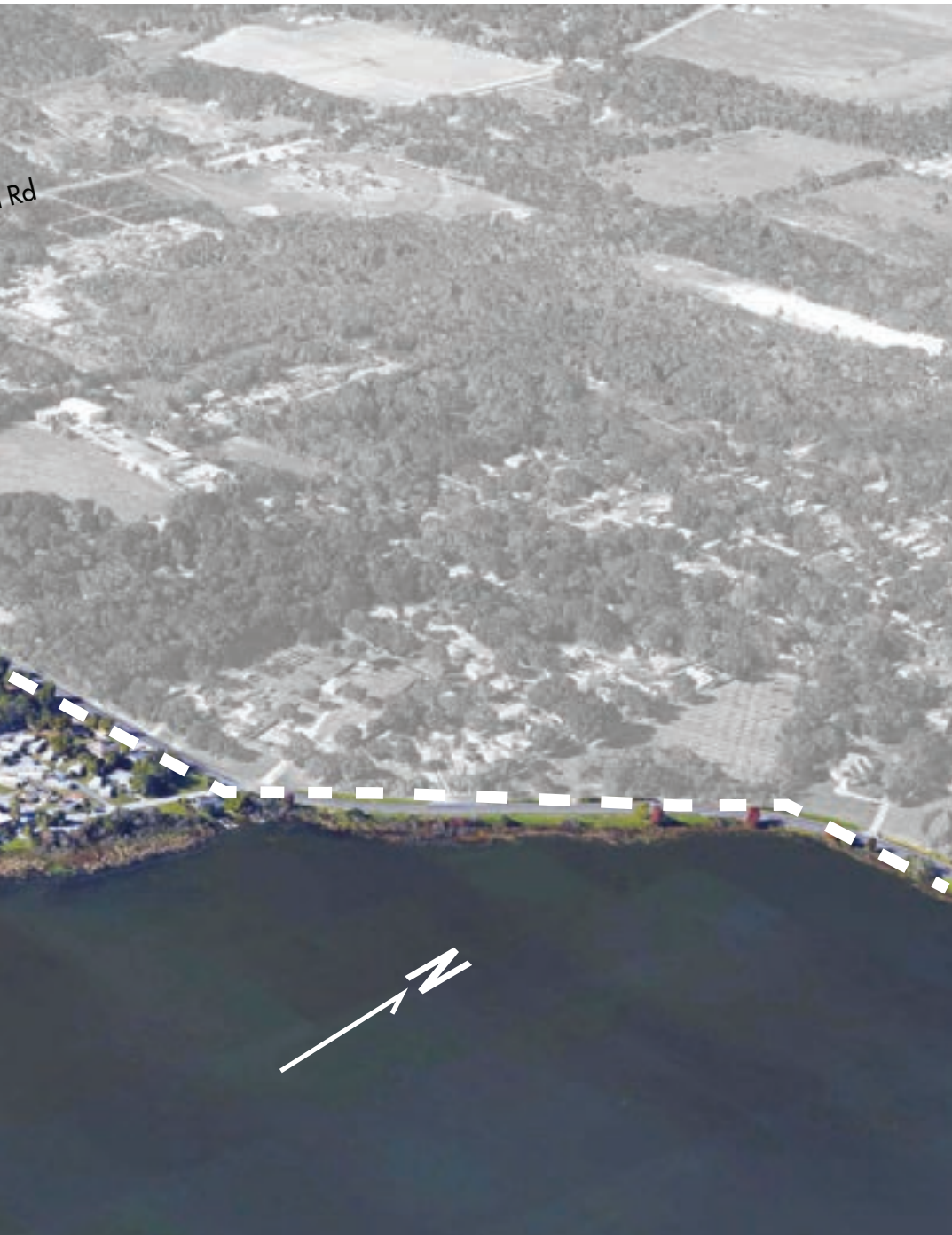
EXPRESSIONS

CLUB ZEPHYRUS



# Study Area Analysis





## 1.1 Study Area

Many components, both physical and non-physical, contribute to the development and character of a place. The purpose of this analysis is to document and characterize the history and context of the downtown from the perspectives of transportation connectivity, land use pattern, building character, infrastructure, and planning policy. These perspectives provide valuable insight into the key issues and opportunities that frame the downtown planning process. They also assist in the formation of the Vision and implementation strategies by helping to construct a clearer and more complete picture of the current state of the downtown area.

The study area for the Downtown Master Plan includes the historic center, the lakefront, adjacent residential neighborhoods, and the western industrial area. It is roughly bounded by SR 429 to the west, Starke Lake to the east, Silver Star Road to the north, and Delaware Street to the south. This entire area encompasses approximately 400 acres of land.

## 1.2 HISTORIC CONTEXT

Ever since the 1880's, Ocoee has grown up on the shore of Starke Lake, with its downtown and residential neighborhoods oriented to the water. For nearly a century, the two miles of countryside between downtown Ocoee and downtown Winter Garden were dominated by citrus and other agricultural uses. The western edge of Ocoee, therefore, developed as the industrial "back door" of the community.

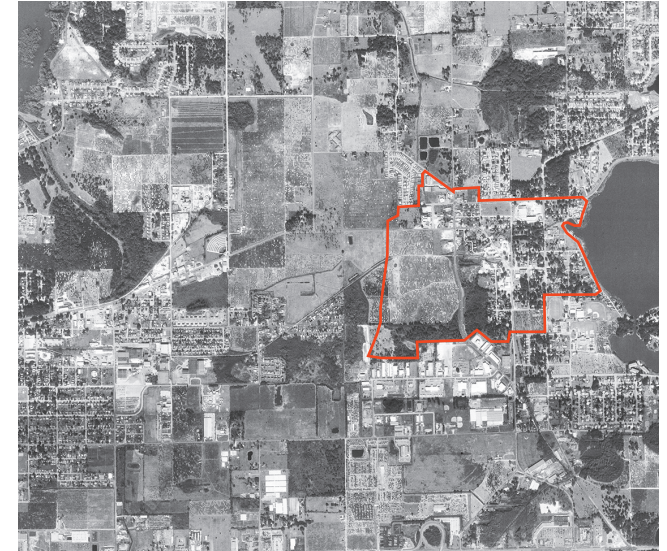
This remained the condition around Ocoee until the 1970's and '80s when new residential development began moving to the north and south of Starke Lake, and away from the traditional downtown. That trend accelerated in the 1990's as Orlando crept towards Ocoee from the east and major investments were made in SR 408, SR 50, and Clarke Road infrastructure. Through the 2000's, the City continued to annex to the north, south, and east, and the downtown moved farther from the "center of mass" of the City.

A major redefining event for downtown occurred in the early 2000's with the construction of the Western Beltway (SR 429) and the Franklin/Plant Interchange. The 429 was routed just west of downtown, through the aging agricultural/ industrial fringe. What was once the back door of the City was now the front door.

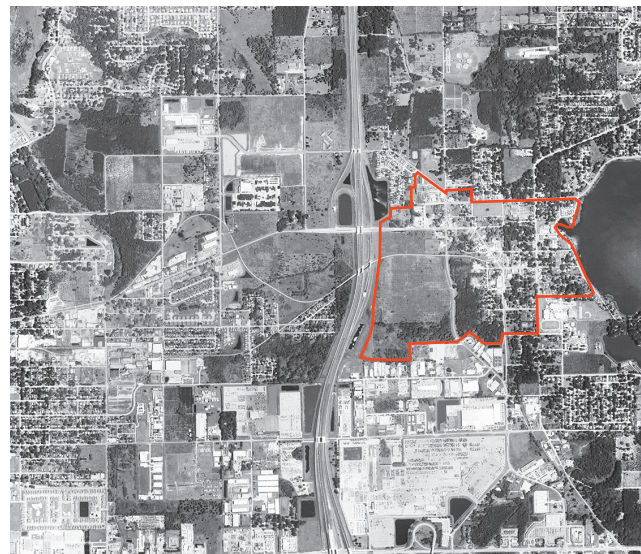
1947



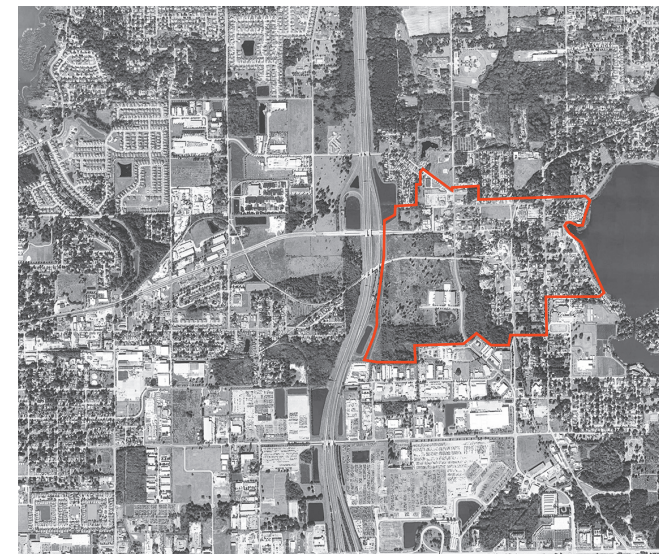
1990



2002



2016



### 1.3 NEIGHBORHOOD CHARACTER



As described in the Historic Context, the character of the western half of downtown is predominantly antiquated light industrial and commercial properties with large, vacant, former agricultural parcels adjacent to the 429 interchange. The area has a distinctly rural character with open drainage swales, unpaved driveways and parking areas, and mix of low density development and vacant land.



The historic main street of Ocoee is essentially the two blocks of McKey Street between the railroad tracks and Bluford Avenue. The character here is a mixture of historic retail structures oriented to the street, more modern retail with parking in the front setback, and repurposed homes and garages. Previous streetscape improvements on McKey have sought to establish a consistent character for the public realm between these two segments with sidewalk and on-street parking pavers, planters, and furniture.



The character of the residential areas of downtown is dominated by one-story bungalows built between 1920 and 1950 and ranch-style houses built in the 1950's. Scattered among these are several newer homes as well as much older historic homes of various architectural styles. Lot sizes vary widely within the study area and can even vary significantly within a single block. The condition of the housing stock also varies widely throughout the downtown.

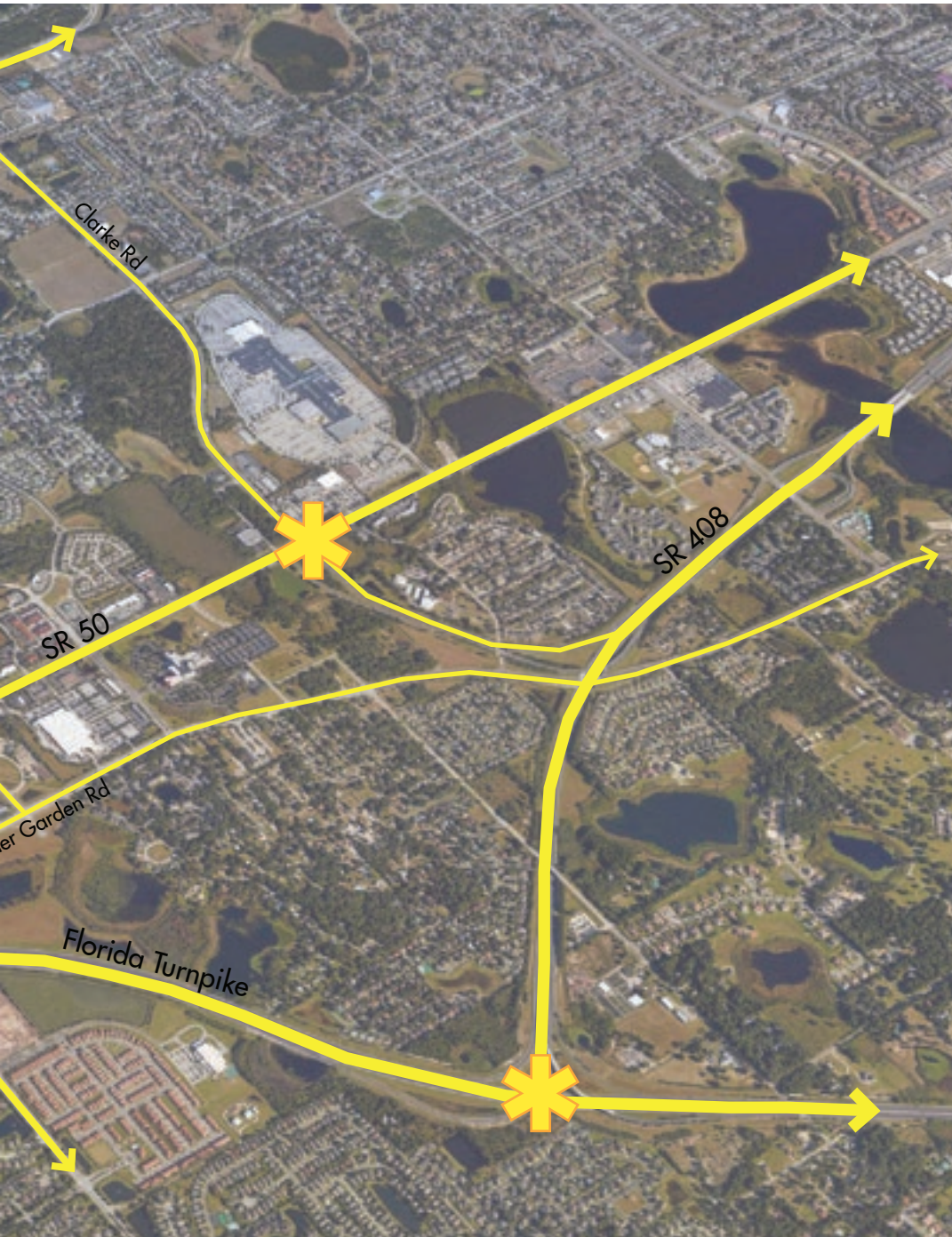


WEST ORANGE TRAIL

DT Winter Garden

DT Ocoee





## 1.4 REGIONAL CONNECTIVITY

On a macro level, Ocoee as a whole has a tremendous amount of regional connectivity. Major regional arterial roads including the Florida Turnpike, Western Beltway, East-West Expressway, and SR 50 crisscross the City around the periphery. Minor arterials, including Silver Star Road, Franklin/Plant Street, Clarcona Ocoee Road, and Maguire/Ocoee Apopka Road provide access to adjacent communities. More local collectors that connect in and around Ocoee are Wurst/AD Mims Road, Clarke Road, Story Road, Orlando Avenue/White Road, and Bluford/Lakewood Avenue.

On a micro-level, regional connectivity from any given point, including the downtown, can be challenging. Many of the collector roads do not connect through to arterials and form "T intersections" with other collectors. Some arterials, such as Franklin/Plant Street and Silver Star Road do not directly connect. The 429 acts as a barrier to east-west connections. All of these conditions cause a disjointed network.

The West Orange Trail is a significant amenity and regional connection for west Orange

County, but it is not easily accessed from downtown Ocoee. The trail runs through the northern part of the City but skirts the downtown to the west of the 429. There are no bike or trail facilities that connect the West Orange Trail to potential users in downtown, east Ocoee, or south Ocoee.

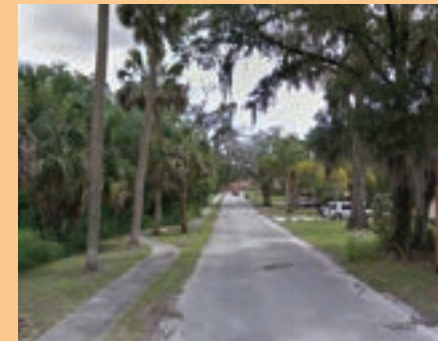


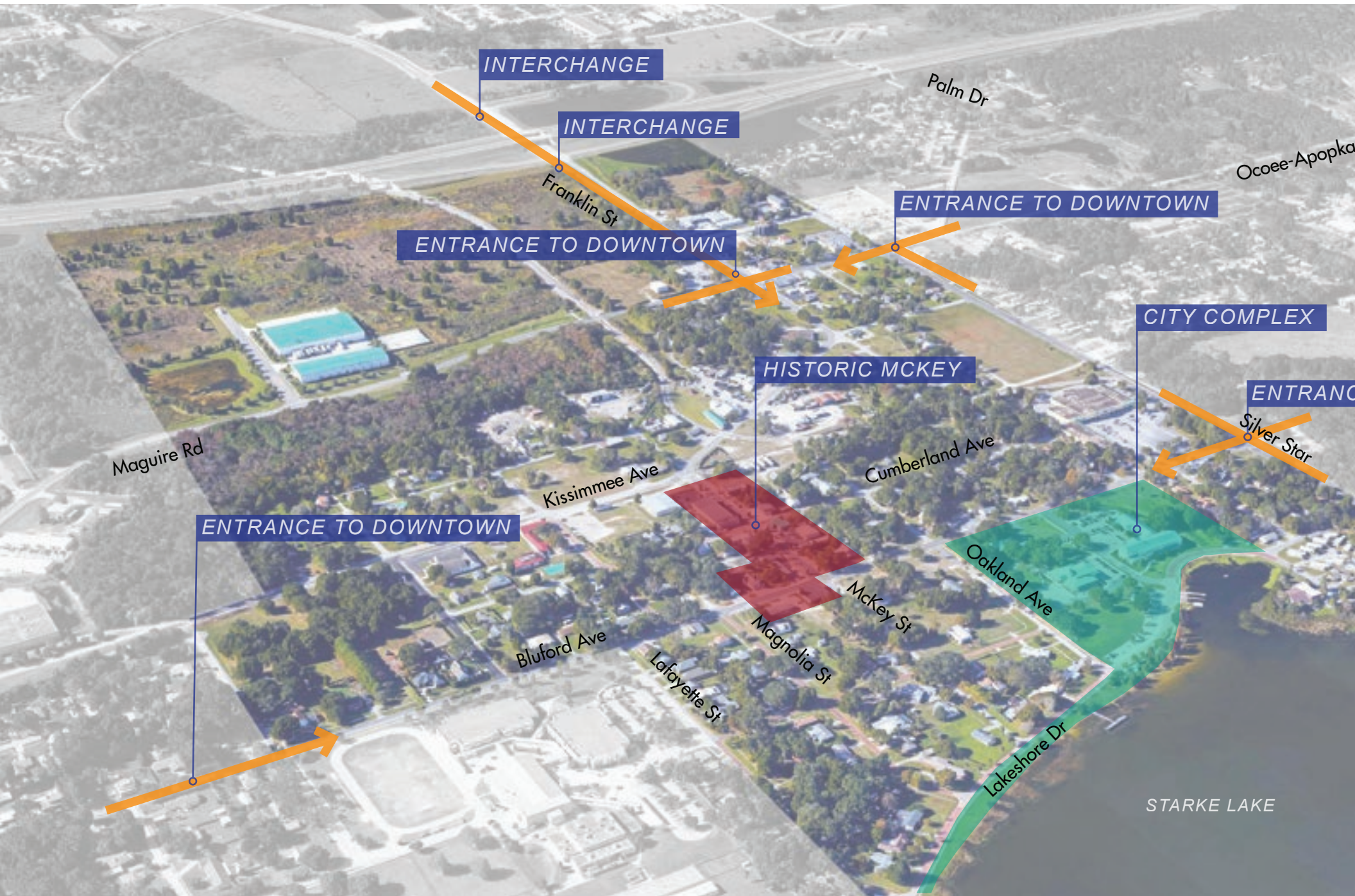


## 1.5 ROAD NETWORK/BLOCK STRUCTURE

The road network and block structure internal to downtown are generally characterized by the historic grid of streets. For a variety of reasons, including the railroad, wetlands, large civic blocks, and prior development, many of the roads do not connect through to the peripheral collectors or arterials. Key road segments in multiple locations have been removed or never existed, resulting in a disjointed internal street network.

Despite the disjointed network, most of the pedestrian-scale historical block pattern of downtown still exists. With a few exceptions, blocks are rectangular, with lengths between 300 and 600 feet, and areas between 2.5 and 4.5 acres. Taylor Street, which is the traditional link between Franklin Street and McKey Street, is a diagonal road, and thus creates irregular blocks. Parcels along Taylor do not align north-south like the other parcels downtown.





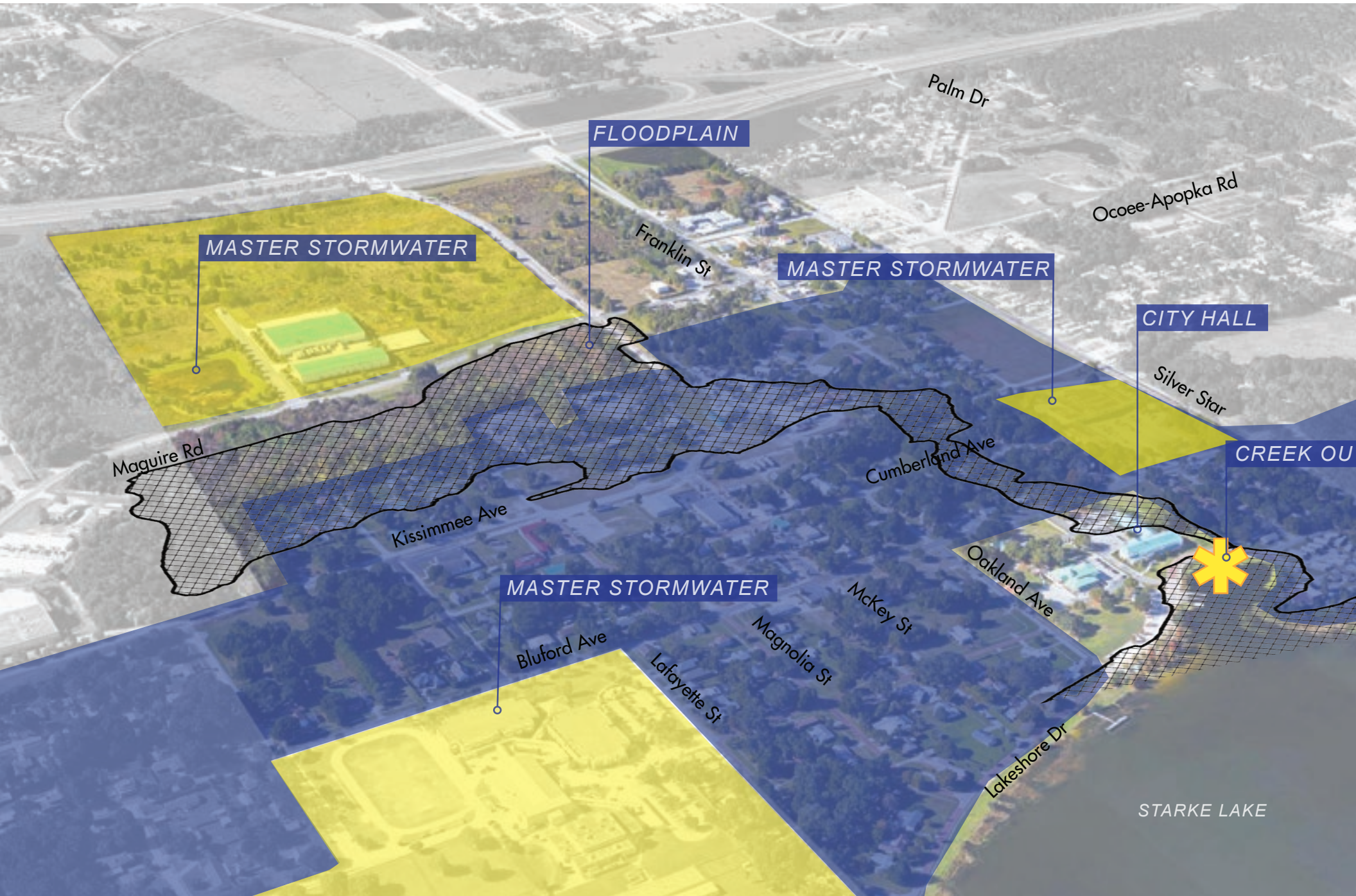


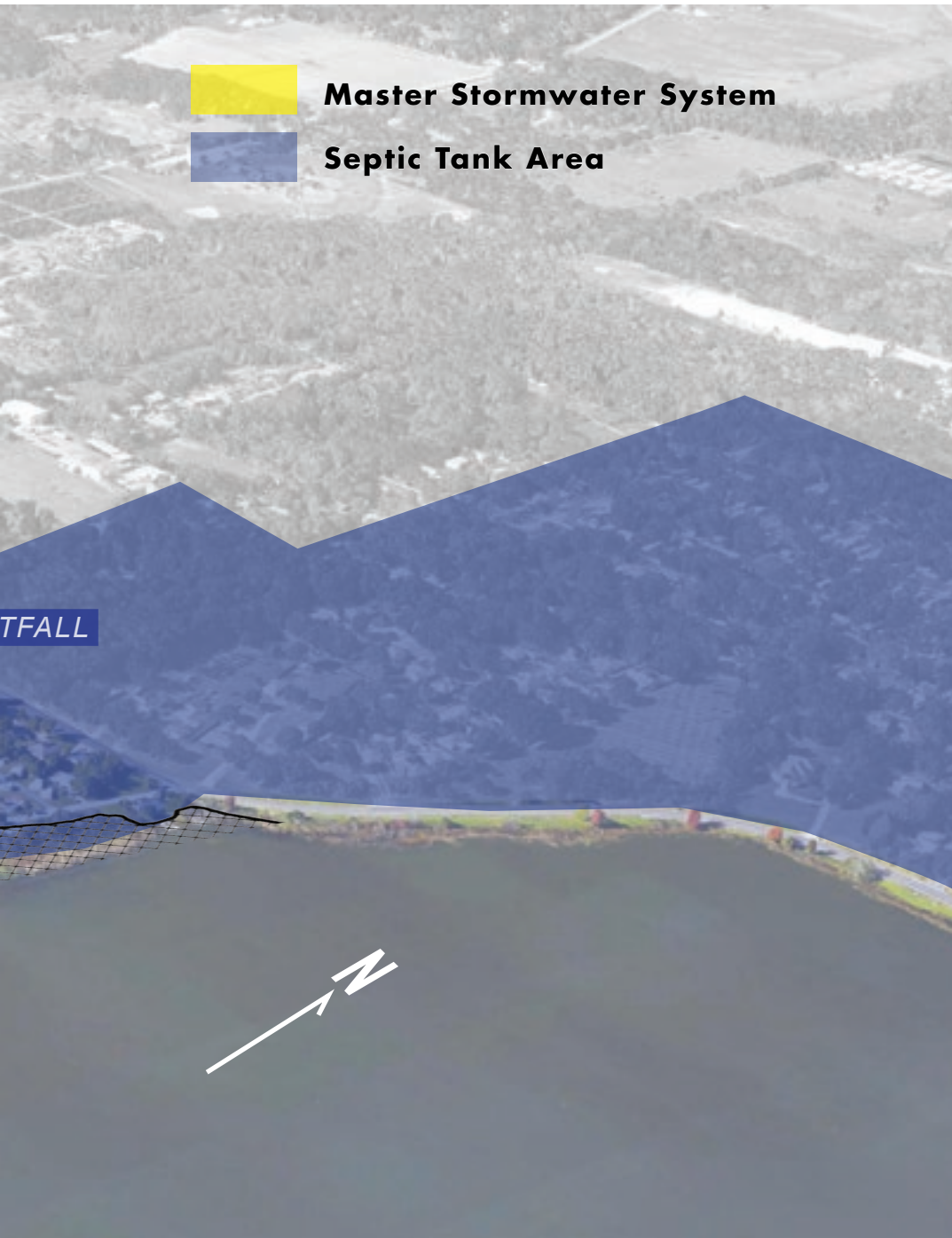
## 1.6 VISIBILITY AND ACCESS

Even with what seems like a robust regional road system in Ocoee, downtown itself still feels somewhat isolated and difficult to access from the region. As mentioned in the Historic Context, the main reason for this is that downtown Ocoee is a downtown of back doors. All the major through streets such as SR 429, Maguire Road, and Silver Star Road only touch downtown on the periphery. Even Bluford, which could be considered a front door to Ocoee, runs perpendicular to the historical main streets and is not a significant regional connection.

Similar to the access issue, downtown has little visibility from the surrounding road network. Some minor signage into downtown exists on Bluford Avenue, however, there is not a comprehensive wayfinding program routing users into and around downtown. Though there are Ocoee gateway features elsewhere around the city, opportunities for gateways into downtown have not been taken. There are many opportunities for improving visibility of downtown from the surrounding areas.







**Master Stormwater System**



**Septic Tank Area**

## 1.7 INFRASTRUCTURE

### 1.7.1 Roads

All roads in downtown are two-lane, typically with 10 foot travel lanes or less; however, in many cases the available right-of-way is much larger than the road – 60 to 80 feet in some cases. Road conditions vary widely. Several segments of Oakland Avenue, Cumberland Avenue, Floral Street, Lakeshore Drive, Magnolia Street, Lafayette Street, and Lakewood Avenue still have their original brick pavers. Much more information on existing road sections is provided in Sec. 3.11.5.

### 1.7.2 Sidewalk

Sidewalks throughout downtown are absent or substandard. Where sidewalks do exist they are too narrow, end mid-block, are in disrepair, and/or have no ramps at intersections. Marked crosswalks are almost non-existent. Along McKey Street, the sidewalks were constructed with pavers during a previous streetscape project, but over time the pavers have become uneven and are difficult for elderly and handicapped to use. Adequate sidewalks must be included in all future streetscape projects and typical street sections, so that as streets are improved or replaced, a safe network is created that is accessible to all users.

### 1.7.3 Sewer Service

Sewer service is currently not available in the historic sections of downtown

and all properties are on septic tanks. Generally, only properties along Maguire Road and north of Silver Star Road have access to the City's sewer system. This is a major hindrance to the redevelopment of downtown and contributes significantly to the nutrient loading of Starke Lake. Major sewer expansion projects into downtown must be incorporated into the Downtown Master Plan to improve existing conditions and accommodate future growth.

### 1.7.4 Stormwater

Most stormwater runoff from the downtown sheet flows into the creek and out to Starke Lake. Some sections of Bluford, Oakland, McKey, and Cumberland have gutter and stormwater inlets that drain into the creek or directly to the lake. Only the middle school, Ocoee Plaza, and Ocoee Business Park have on-site stormwater ponds. This lack of stormwater treatment is a significant source of pollution into Starke Lake and any improvements could have major benefits for the health of the lake and its wildlife; however, the small block structure of downtown and scattered ownership of properties makes it difficult for individual redevelopment properties to provide their own on-site stormwater. A master stormwater system for downtown could be an effective option to both promote redevelopment and improve the water quality of Starke Lake.

## 1.8 THE LAKEFRONT







### 1.8.1 City Hall Complex

City Hall was constructed in the mid 1990's and contains approximately 32,000 square feet of space. Portions of the building are structurally un-sound due to insufficient foundation preparation and will likely require City Hall to be relocated or rebuilt in the near future. If the building is relocated, that will leave an opportunity for some other use to be put in its place utilizing appropriate foundation preparation.

### 1.8.2 Lakefont Park

The Lakefront Park is the collective name for several community facilities adjacent to Starke Lake and City Hall, including the Lakeshore Center, Bill Breeze Field, Withers-Maguire House, and lakeside.

#### **Lakeshore Center**

The Lakeshore Center (fka Ocoee Community Center) was constructed in 2014 and is Ocoee's premier civic center space. The facility features nearly 5,000 square feet of rental space and is an extremely popular venue for weddings other events. There are plans to expand the Lakeshore Center's facilities, including additional event space.

#### **Bill Breeze Park**

Bill Breeze Park occupies the space between the City Hall complex and Lakeshore Drive. It is the hub

of many community-wide events such as Founders Day, Spring Fling, movies in the park, food trucks, and much more. The park is approximately 2 acres in size and features an open lawn, gazebo with seating, public restrooms, and boat trailer parking.

#### **Withers-Maguire House**

The Withers-Maguire House is an historic house built in 1888, and is on the National Register of Historic Places. It is used as a museum and event space.

#### **Lakeside**

Between Starke Lake and Bill Breeze Field is a strip of park space where Ocoee residents have intimate access to the water. The lakeside features a boat launch, covered picnic pavilions, uncovered picnic tables, benches, fishing pier, and multi-use path. Starke Lake is a "Fab-Five" fishing lake and is regularly stocked and managed for sport fishing. This makes the Lakefront Park an extremely popular spot for boaters and anglers from Ocoee and beyond.

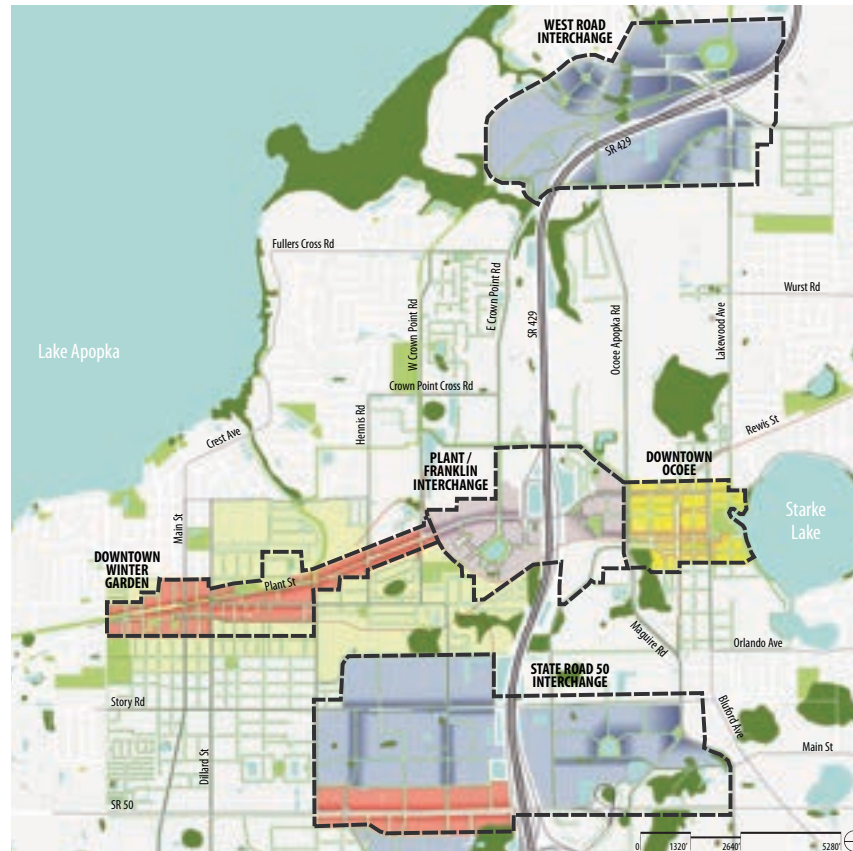
#### **Ocoee Women's Club**

The Women's Club is an art-deco style building built in 1938. It has approximately 2,500 square feet of multi-purpose meeting space. The facility is adjacent to .

## 1.9 REGULATORY ENVIRONMENT

### 1.9.1 SR 429 Vision Plan

The SR 429 Land Use and Economic Development Study (429 Plan) was created out of the common interests of the cities of Ocoee and Winter Garden to plan for the future of the three interchanges they share and their respective downtowns. For downtown Ocoee, the plan identified the three biggest opportunities: to improve road and trail connections into downtown, to “reinvigorate the heart” with improvements to the parks and civic spaces, and to create a mix of uses – all of which would bring more people downtown and make them want to stay longer. The 429 Plan and the positive future vision for downtown Ocoee that it displayed became a major impetus for the City to move forward with a full-blown downtown master plan.



## OVERALL MASTER PLAN LAND USE, CIRCULATION, AND OPEN SPACE

### GOALS

**Guide the Transition of the Area From A Back Door to A Front Door**  
Prior to the construction of SR 429, this corridor was truly the back of both communities, where more industrial uses were located out of the way of the premium locations in town. SR 429 and its three interchanges have flipped this situation, and now presents the opportunity to create new premium spots for development.

### Create Places for Opportunity

Both cities need to have locations available for catalytic activities and private investments. This goes beyond just having land; it extends to utilities, entitlements, and an underlying public realm that is well-connected to the rest of the community and will attract this type of investment.

### Make the Place Seamless

Developers and others should not perceive a difference between the two cities along the corridor. This idea translates to land use, permitting, zoning, and aesthetics.

### Be the Location of Choice on SR 429

Rather than a location of convenience, the goal of the community is to attract the highest-quality development possible. Doing this requires a high standard for new development with regard to land uses, buildings, and the public realm.

### PLANNING PRINCIPLES

In order to reach these goals, development in the area should rely on four core planning principles:

**A Livable Transportation Network** – people-oriented transportation focused on a network of street connections rather than a limited system of large roads

**A Strong Sense of Place** – building on the existing character of these West Orange County communities

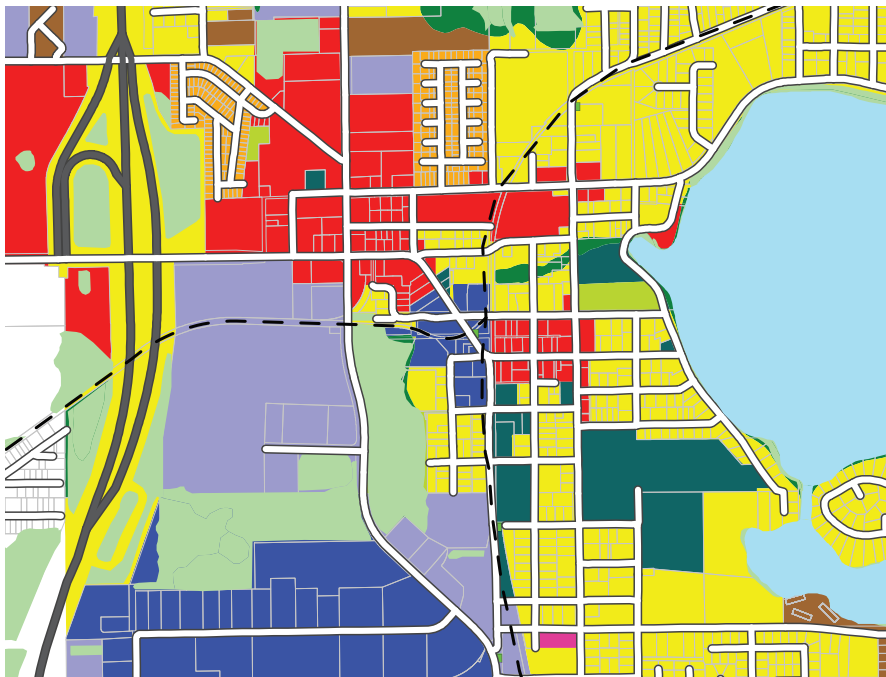
**Mixed and Multi-Use Development** – to create more activity at more times of the day more days of the week to ensure a vibrant level of exchange

**A Robust Public Realm** – linking the community with parks, trails, and open spaces

### 1.9.2 Comprehensive Plan

The City's Future Land Use designations in the study area include Light Industrial, Heavy Industrial, Commercial, Low Density Residential, Public Facilities, Recreation and Open Space, and Conservation. In addition to these designations, a portion of the western Study Area is within the Interchange Impact Area – defined as at least ¼ mile distance from the interchange. The Comp Plan encourages mixed and multi-use projects in this area and discourages strip commercial and fast food. In general, the FLU designations follow the long-established existing uses.

Future Land Use Map

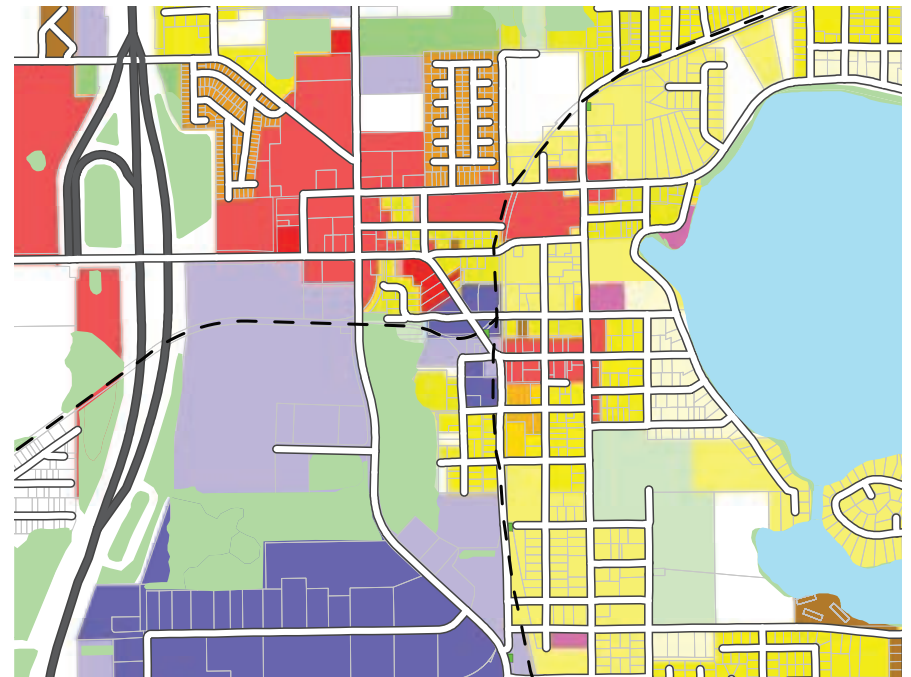


### 1.9.3 Zoning

The City's zoning standards are generally suburban in nature and are not appropriate for redevelopment in the context of a historic downtown. What works fine for large green field properties along arterial roads does not work well on the small lots and small blocks in downtown:

- Minimum lot sizes are too large, setbacks are too wide, parking ratios are too high, etc.
- Mixed use may be allowed in commercial zones with special exceptions, but there are insufficient guidelines for implementation.
- The location of retail and office uses is limited to the Silver Star corridor and portions of McKey Street. It is clear that there are other locations where retail/mixed use is desirable.
- There are no special regulations regarding building architecture to enhance and protect the existing character of the place.
- A zoning overlay that focuses on the specific needs of development in downtown is a way that more appropriate standards can be implemented for Ocoee.

Zoning Map





GATEWAY

VACANT LAND

VACANT LAND

VACANT LAND

VACANT LAND

VACANT LAND

CITY HALL

GATEWAY

TO WEST ORANGE  
Ocoee-Apopka Rd

TO WEST ORANGE  
TRAIL

TO WEST ORANGE  
TRAIL

Silver St

CREEK

STARKE LAKE



## 1.10 OPPORTUNITIES

There are several significant opportunities that have been identified solely on the results of the Study Area Analysis, many of which were later validated and expanded on during the public visioning process of the Master Plan. These opportunities are described briefly below.

**Vacant Land** – Vacant land is low-hanging fruit for new development and could be potential catalyst sites that spur additional growth around it.

**City-Owned Properties** – Properties that the City already owns can be used for a variety of purposes to spur new growth including new park space or civic buildings, parking, or infrastructure improvements.

**Large Land Owners** – Large contiguous areas under single ownership provides for potential game-changing developments or redevelopment to occur since there is no need for costly parcel assembly.

**Exiting Parks** – The need to eventually move City Hall provides an opportunity to expand and improve the already greatly valued Lakefront Park.

**Creek** – The creek and surrounding land is an opportunity to provide enhanced stormwater retention, greenspace, and improved water quality into Starke Lake.

**Access** – There are many opportunities to improve access in and around downtown. Silver Star Road will eventually be improved by FDOT and extended to the Franklin/Plant interchange. Bluford Avenue has already been improved south of downtown, and the segment through downtown will be improved next. Opportunities exist to extend Oakland Avenue and McKey Street. There are even several opportunities to bring connections to the West Orange Trail into downtown.

**Gateways** – With improvements to the surrounding roads come opportunities for new gateway features and other signage announcing downtown.





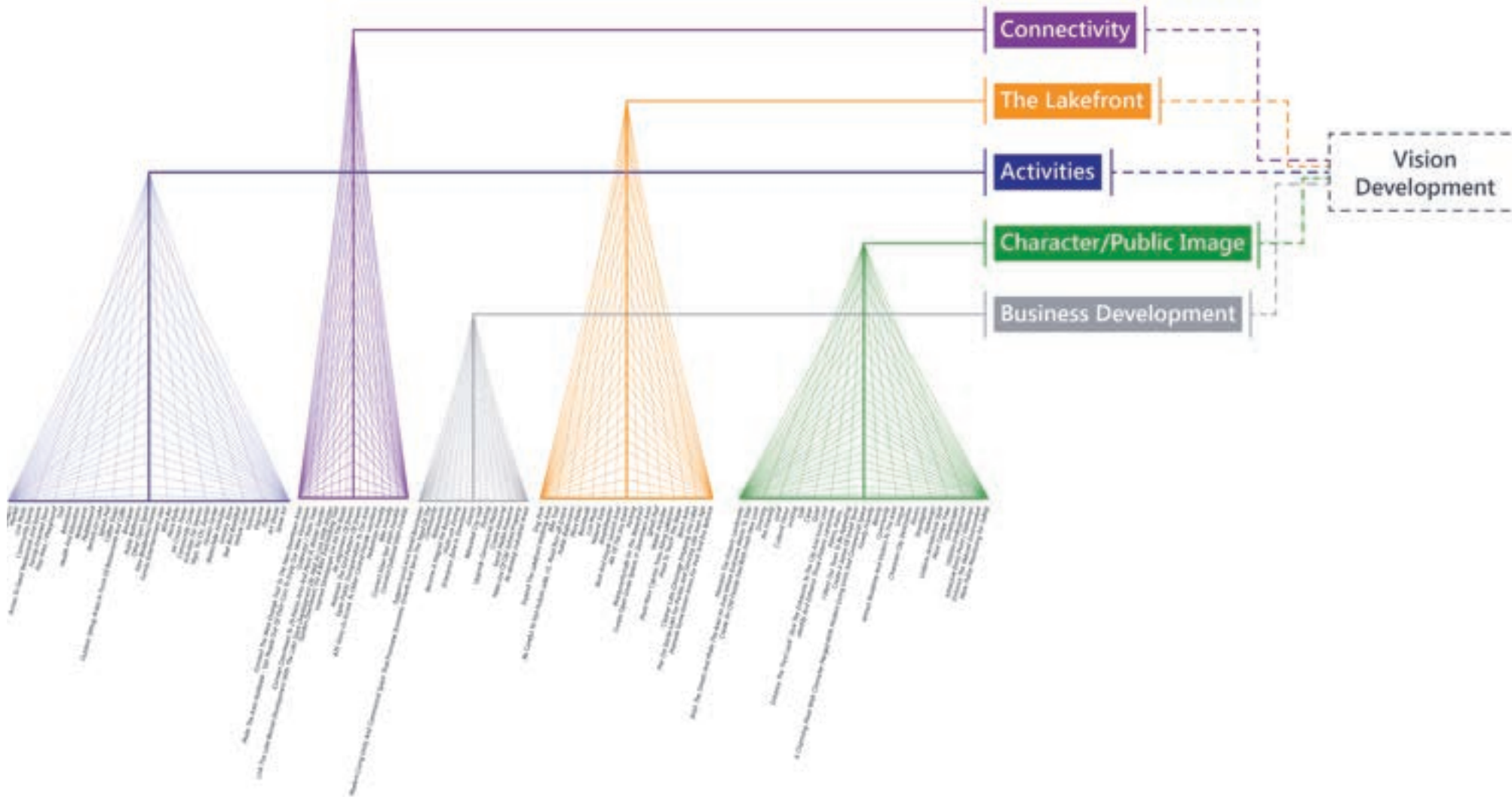
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## Master Plan Vision

# Planning Framework

Listening and Learning: Public Kick-off Meeting & Walking Audits

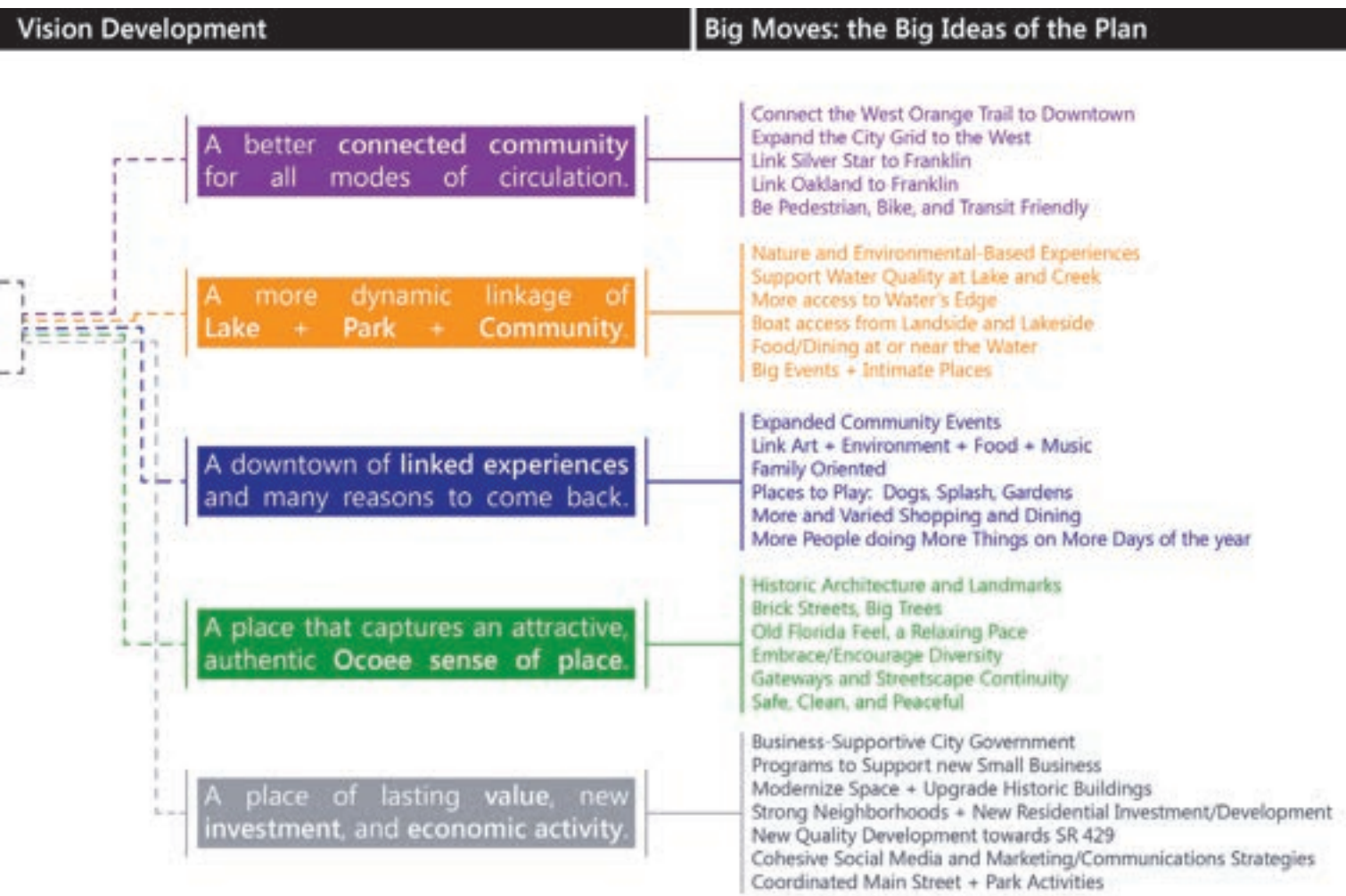
Citizen-Based Themes





## 2.1 DEVELOPING THE FRAMEWORK

The Master Plan is based on a set of Citizen-Based Themes developed from the massive amount of public input gathered over the planning process. Though the citizens' opinions regarding the good, the not so good, and the opportunities in downtown were diverse, the majority coalesced around five themes: Connectivity, the Lakefront, Activities, Character/Public Image, and Business Development. These themes were each expanded into several Big Moves, that in-turn lead to projects to accomplish the Big Moves. More information on the formulation of these themes and the public process that went into developing them is provided in the companion document *Understanding: Listening and Learning Summary*.





Palm Dr

Silver Star Rd

Lee St

Taylor St

Ocoee-Apopka Rd

Oakland Ave

Maguire Rd

Bay St

Floral St

Kissimmee Ave

Ohio St



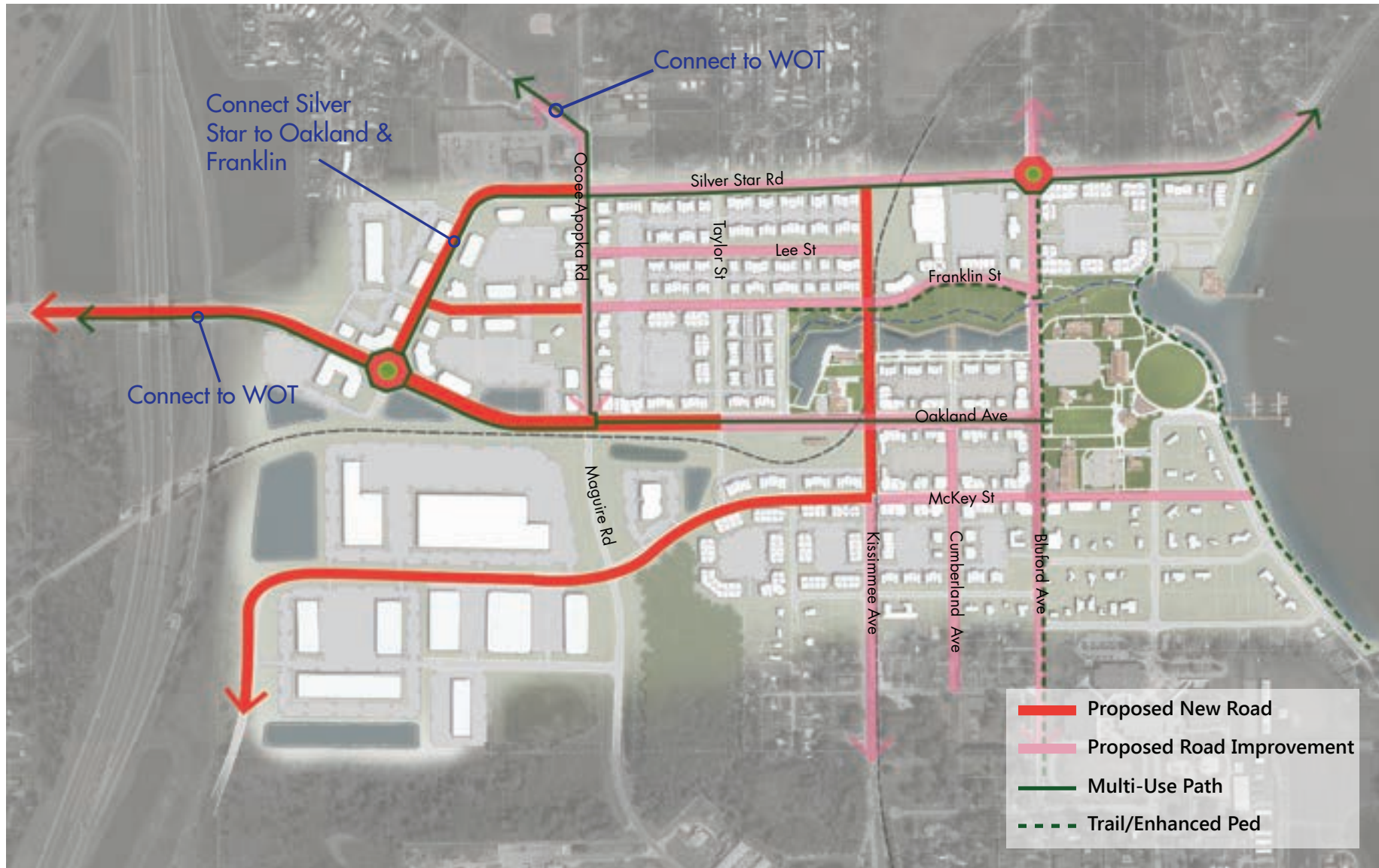
## 2.2 LONG-TERM MASTER PLAN VISION

Using the mass of public input about what a re-captured, reinvigorated, and authentically Ocoee downtown would be, we were able to create a Long Term Master Plan Vision (*left*). It visually embodies the people’s far reaching aspirations for the future of their town.

The Vision Framework on the following pages is composed of the Big Move ideas for each Citizen-Based Theme generated by the public input processes. Each Big Move is a statement of the desired future condition of downtown, as articulated by residents and business owners. Some Big Moves are very specific, regarding particular types of physical improvements or investments to be made. Others refer more generally to the type of downtown experience we want to have.

Though this plan primarily addresses the built environment of downtown, even the less tangible Big Moves will be impacted to some extent. The Frameworks on the following pages describe the major concepts of the Big Moves that contributed to the formulation of the Master Plan Vision and Code Overlay.

## 2.2.1 Connectivity: A Better Connected Community for all Modes of Circulation



## VISION

*This theme is born out of the general consensus that downtown is not well connected to the region and much can be done to improve access for automobiles and bikes. It is also necessary to improve the internal circulation of downtown to allow freer movement of pedestrians, as well as automobiles and bikes.*

### Big Move: Connect the West Orange Trail to Downtown

The West Orange Trail (WOT) has been a transformative catalyst for multiple communities around Ocoee, but downtown is too far away and has no direct connection to reap any of the benefits the WOT can bring. One Big Move that was very clearly articulated by the citizens of Ocoee is that downtown must have a spur connection to the WOT.

Three major options for a spur to get to downtown from the WOT are available: Ocoee Apopka Road, Palm Drive, and Plant/Franklin Street. The current decision for the Big Move is that all three should have enhanced bike facilities, but that Palm Drive should be the official WOT Spur. Though Palm Drive is less direct and has more road crossings than Franklin Street or Ocoee Apopka Road, it is a much shorter route and has far less automobile traffic.

Once into downtown, the WOT spur will run in the center of Oakland Avenue towards the lakeshore. Oakland Avenue has a ROW of 80', which is significantly wider than any other downtown road,

making it ideal for this purpose. McKey Street was another option for the trail spur, but the ROW is only 60' and is already maxed out by the existing street and adjacent buildings.

### Big Move: Reconnect and Expand the City Grid

Downtown Ocoee has a traditional grid of streets that links it internally and with the surrounding community. One of the main Big Moves contemplated in this Master Plan is to make additional east-west connections by expanding the grid to the west and filling in missing links where possible. Two major east-west links considered are Oakland Avenue and McKey Street, which today, end at Taylor Street. In the Master Plan, these two major downtown streets are extended to the west past Maguire Road to provide more direct connections into downtown. Other moves related to the grid are to straighten the Taylor Street diagonal into a squarer block and add the missing segments of Kissimmee Avenue.

### Big Move: Link Silver Star to Oakland and Franklin

Silver Star Road is a major east-west thoroughfare, connecting areas east of Ocoee to SR 429, but it does not directly connect to Franklin Street or the interchange. To make the final connection to the interchange from Silver Star Road, one must turn south for two blocks and then west onto Franklin Street. This configuration functions, but is not ideal for access to or from the 429.

The Big Move from the Master Plan is to realign the western terminus of Silver Star Road to bend south to intersect Franklin Street directly and meet the extension of Oakland Avenue. In this Vision, the Oakland extension becomes the main route into downtown from the interchange, where today the main way is Franklin-Taylor-McKey route. If the FDOT and traffic volumes permit, the intersection of Silver Star Road and Oakland Avenue could be an opportunity for a roundabout.

### Big Move: Be Pedestrian, Bike and Transit Friendly

The final Big Move is possibly the most important, as it demands a shift in the status quo in downtown and actively elevating these other modes of transportation much higher than in the past. The goal is to foster safer routes to school,

entice more people to come into downtown and stay, and reduce the need for automobile travel. To this end, all road and connectivity projects proposed in this master plan have significant components for bikes and pedestrians, as shown in the typical road sections in section 3.11.





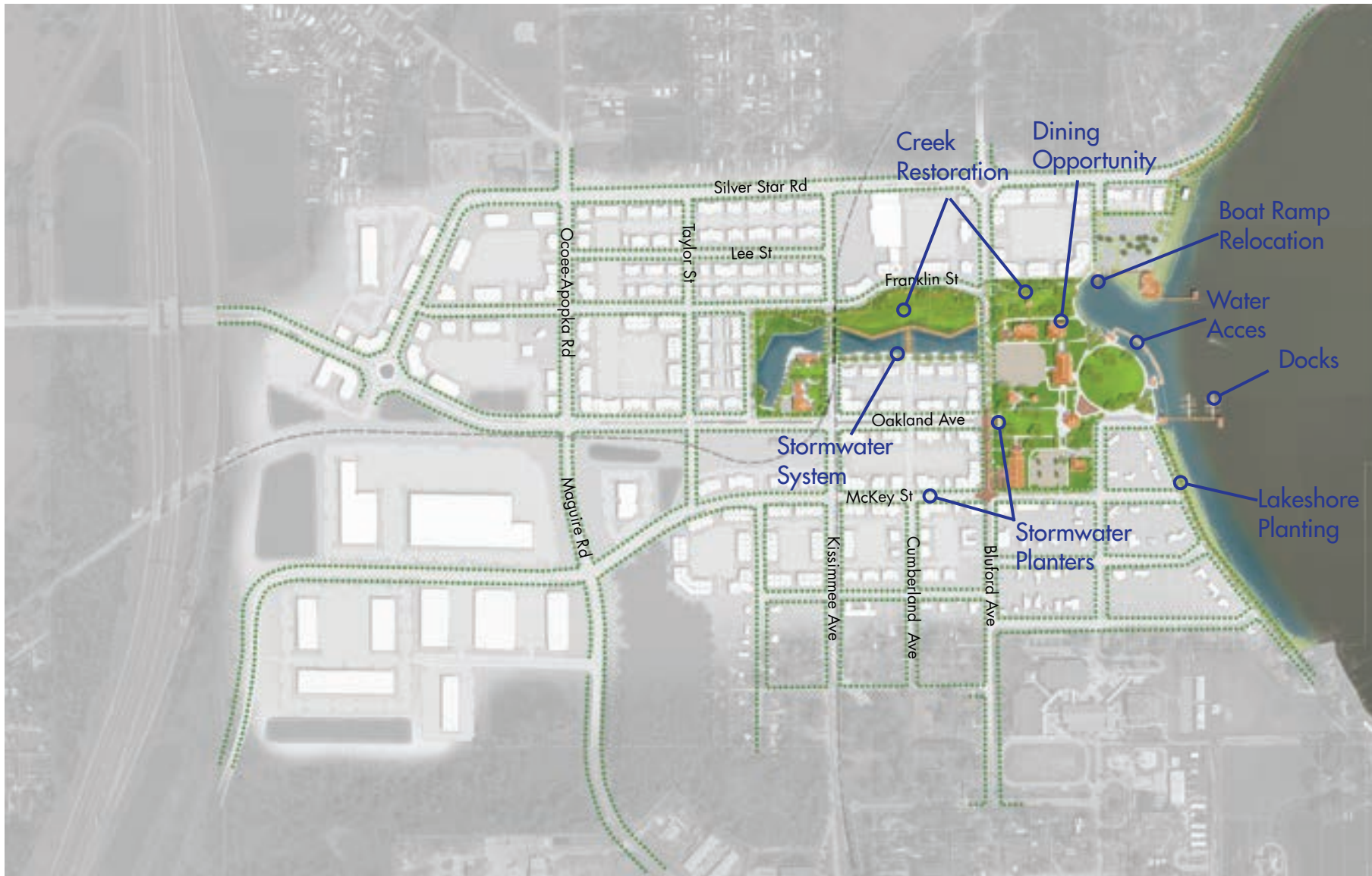
## PROPOSED OAKLAND AVENUE

Oakland Avenue is such an important place for Ocoee, with incredible potential to transform the downtown. It's time Oakland accepted the role it was meant to play.

The rendering to the left depicts an early vision for Oakland Avenue. It clearly illustrates how the Oakland Avenue ROW can accommodate a trail in the median, on-street parking, and wide sidewalks. The final design of the future Oakland Avenue will be in accordance with the typical sections provided in

Section 3.11 of this report. Note that the development shown is conceptual only. It does not represent specific developments proposed by the City, only how buildings should address the street.

## 2.2.2 Lakeshore: A More Dynamic Linkage of Lake+Park+Community





## VISION

*There is much more to include in this Framework than simply the strip of land adjacent to the lake. This topic describes how the natural environment, which is anchored by the Lake, can and should be enhanced by better linkages into the community. It encompasses the Lake, the downtown as a watershed, the parks and civic spaces, and how people access them. A common term used for this concept is Green Infrastructure because, like water, sewer, and roads, the natural realm itself forms a framework that binds a community together. The following Big Moves describe the Green Infrastructure framework described by the Master Plan.*

### Big Move: Support Water Quality at Lake and Creek

Water quality in Starke Lake is critical to maintaining the health of fish and wildlife, and as a major local fishing destination and Fab Five Lake, it is a significant economic issue as well. The major hurdle to improving the water quality in the lake is the lack of sewer utilities and untreated stormwater runoff – and downtown is a major source in both categories.

The actions involved in this big move are three-fold:

- Expand the City's sewer system into downtown and eliminate the use of septic tanks;
- Construct a master stormwater system for the downtown; and
- Take a holistic approach to stormwater runoff upstream of the creek utilizing stormwater planters, pervious pavers, and other best management practices.

### Big Move: Nature and Environment-Based Experiences

The keys to making nature accessible is to improve the health of the existing natural elements, actively create new experiences as part of ongoing public infrastructure and civic projects, and eliminate physical barriers to public access. Ocoee has many opportunities in all these areas. A few important ones are: enhancing the lakeshore with native plant materials and removing exotic species, taking advantage of the controlled water level with flush boardwalks, and restoring the creek/drainage ditch to a more natural condition and enhancing it as an amenity.

### Big Move: Big Events + Intimate Places

The proposed park expansion is large, but it is broken up into smaller, more intimate spaces to accommodate a wide variety of

events and uses. It should be equally effective at hosting mega events like Founder's Day, as it is at hosting medium events like food truck/movie night, and small events like weddings. The park is divided into several size plazas and lawns to give the spaces maximum flexibility while still maintaining a cohesive park character and flow.

### Big Move: Boat access from Landside and Lakeside

The existing boat launch and trailer parking in the Lakeshore Park is not ideal. The boat launch facilities and Lakeshore Drive create a physical separation between the shore and the park and is a very active use in the heart of a passive-use area. The Master Plan describes a vision where Lakeshore Drive is closed as a public street, the boat facilities are moved (possibly to the north), and the park is expanded to the shore. In addition to the launch area, a new dock to the south will have facilities for boats to tie up, so residents from the opposite side of the lake can boat to downtown as easily as they could drive.

### Big Move: Food/Dining at or near the Water

Many waterfront communities across central Florida and the country have some kind of feature dining experience adjacent to the water, where one can come and get food and drink, with the water as

a backdrop. Ocoee, however, has surrounded their waterfront with civic and residential uses. Many residents have expressed a desire for some waterfront dining, as long as it is consistent with the character of the lakeshore and it is limited in scope. If City Hall does move from its current site, this location would be ideal for a small waterfront restaurant with accompanying bed and breakfast type establishment. This, combined with the Lakeshore Center and Withers-Maguire House, would form a sort of retail and event campus in the midst of the park.





## LAKEFRONT PARK

The lakefront is the cultural, recreational, civic, and environmental heart of the city. Ultimately, the fate of downtown will rest on the ability to harness and activate it.

This is a conceptual rendering of a revamped Lakefront Park from the viewpoint of a boater on Starke Lake. In the image, you can see the formalized lake

edge and native plantings, swing pavilion and boardwalk, creek improvements, and restaurant in the current City Hall location.

## 2.2.3 Activities: A Downtown of Linked Experiences and Many Reasons to Come Back



## VISION

*The economic success of any downtown hinges on its ability to attract visitors, extend their stay, and make them want to come back again and again. This is accomplished by having a variety of linked experiences such as restaurants, shopping, parks, trails, and events. For Ocoee, the Lake/Park experience is vital, but focusing on it exclusively is not the way to grow downtown. People will drive in for the big special event, have nothing else to do or see once it's over, and will leave. The linkages are what enable people to come to downtown, organically add more destinations to their trip, and want to come back again for a new experience. All of the big moves in this framework involve increasing the number of experiential linkages that are available in downtown Ocoee.*

### Big Move: Link Art + Environment + Food + Music

Great downtowns today are those that provide a linked series of experiences related to art, the environment, food, music, and more. Ocoee has many of these experiences, with the lakefront, farmers markets, concerts in the park, food truck nights, and others, but many times they are physically isolated, temporary, or too small to reach critical mass. All these factors keep the downtown experiences from being linked and trips extended. The Downtown Master Plan takes several steps to establish a framework to link these better in the future:

- Expanding and reorganizing the lakefront park area and moving City Hall to McKey creates a much more direct linkage between the historic main street and the waterfront;
- Daylighting and celebrating the creek as an amenity connected to the lakefront, provides a much more extensive environmental experience;
- Improving development standards in downtown encourages the right scale and character of redevelopment; and

- Re-building streets with wide sidewalks, bike facilities, and landscaping creates a desirable pedestrian experience that is more likely to encourage trip extension.

For example, McKey Street, the waterfront, the park, and the Lakeshore Center are all somewhat disconnected from each other, so when an event is held at either McKey Street or the park, few people cross to another. Even though the two venues are relatively close, there is a block of no-man's land with narrow sidewalks between them, and few visual cues connecting them.

One way of expanding community events without physically having more events is by leveraging people's trips when they are already downtown for an event. Giving people more things to do when they are downtown for an event extends their trip.

### Big Move: Expanded Community Events

There are already many community events of various sizes held in downtown every year, from weekly farmers markets to the annual

Founder's Day celebration. In general, these downtown events are held in two places: Bill Breeze/Lakeshore Park and historic McKey Street. Today, these venues are acceptable, but not ideal for hosting a variety of events. McKey Street is a quaint, intimate place for street festivals, but needs a streetscape refresh to improve its functionality. The waterfront campus, including Bill Breeze Park, Lakeshore Park, City Hall, and Lakeshore Center, has been improved incrementally over the years and needs a major overhaul and expansion (as described in detail earlier). These improvements will make hosting Ocoee's traditional lineup of events much better, and will make programming a wider variety of events in the future possible.

### Big Move: Family Oriented

To be family oriented does not mean lots of play grounds. There are many types and stages of families, and a downtown should accommodate the recreation, retail, and transportation needs across a wide spectrum. A "family-oriented" downtown must be safe, be easily accessible, be easy to move around in, and have a variety of experiences. Most of the other big moves in this Master Plan also deal with these topics and are described in detail.

### Big Move: Places to Play, Dogs, Splash, Gardens

The redevelopment of the Bill Breeze and Lakeshore Parks will be an excellent opportunity for the City to incorporate more targeted opportunities for play. The bones of the park redesign, as

shown in the Long Term Master Plan Vision, allow for more active uses to be tucked into spaces throughout. Options could include a playground, splash pad/fountain, dog park, formal gardens, or many others. The final program for the park must be carefully balanced based on the needs of the surrounding neighborhoods, who use the park every day, and the community at large, who comes downtown periodically for special events.

### Big Move: More and Varied Shopping and Dining

As a Big Move, the Master Plan approaches the need for more retail and restaurant uses in two ways. The first way is by creating development standards tailored to the needs of a downtown redevelopment environment. Ocoee's typical commercial and office standards tend to result in suburban-style development, which is not appropriate for downtown with the small lot sizes and small blocks. The new standards will promote redevelopment of a small town character and scale that is so important to the residents.

The second way Ocoee will promote a varied dining experience may only come into play if City Hall is relocated. As described in the Lakeshore section, the current site of City Hall could be redeveloped as a waterside cluster of small restaurants and bed and breakfast.

## 2.2.4 Character and Public Image: A place that captures an attractive, authentic Ocoee sense of place



## VISION

*Great communities have a reputation for being places where people want to live, visit, and spend time in, so Character and Public Image are make or break for a downtown. Both the public realm and the private realm play major roles for defining character. On the public realm side, streets, parks, and civic buildings form the bones of a downtown and can set many precedents for the aesthetic of a place with simply the choice of design palettes. On the private side, the architecture, orientation, and condition of buildings plays a huge roll in defining character, as does the landscaping. These two sides play and influence each other in unique ways in every downtown, and the successful ones find a way to make that interaction organic and authentic. For the residents of Ocoee, this topic is of major importance, both to acknowledge the past and chart a path into the future.*

### Big Move: Gateways and Streetscape Continuity

Downtown suffers greatly from a lack of consistent streetscape and wayfinding design. Thus, the downtown itself is not well defined as a place, with most people only identifying McKey Street or City Hall complex as downtown. By creating a consistent palette of streetscape components and wayfinding options, the downtown can be stitched together visually street-by-street. This Master Plan proposes typical street sections for multiple streets around the downtown to provide this continuity.

Gateways are another opportunity for the downtown to establish continuity, both in geography and in aesthetic. They can announce the limits of downtown and provide a visual theme or cue that can be carried throughout the district. There are several opportunities for gateways into downtown, as

shown in the map on the left. If roundabouts are used at the Bluford/Silver Star and Silver Star/Oakland intersections, these could be ideal opportunities for striking gateway features as centerpieces.

### Big Move: Safe, Clean, and Peaceful

Though a downtown must have lots of life, vitality, and activity to be successful, it is important to remember that people live downtown, and houses immediately surround the most active areas. In pursuing a more vibrant downtown, it must remain fundamentally, a small town where people want to shop, work, recreate...and live. The City is mindful that issues of density, traffic, parking, noise, litter, and public safety are all factors when considering redevelopment, public events, and capital investments.

### Big Move: Old Florida Feel, a Relaxing Place – Brick Streets and Big Trees

Many residents of Ocoee strongly identify the City with a timeless aesthetic reflected in old-Florida architecture, brick streets, large trees, and a relaxed pace; however, the desire for new growth in the downtown is paired with the concern that growth will displace this traditional character that is so appreciated about Ocoee. So, to protect and enhance this character in downtown, the Big Moves for the Master Plan here are several-fold:

- Enact design standards that ensure the character and urban form of new downtown development is consistent with existing and desired character;
- Require new civic buildings to maintain and enhance the old-Florida aesthetic;
- Enact tree protection regulations and landscape requirements that enhance the historic tree canopy;
- Incorporate new street trees in road construction projects; and
- Strategically incorporate brick into streetscape projects, either in the street, as accents, or in on-street parking stalls.

### Big Move: Embrace/Encourage Diversity

Today's Ocoee is a very diverse place, with a diverse population and diverse housing and retail needs. To accommodate this diversity, downtown can and should be an accessible and desirable place to live for any price-point and family type. The Master Plan and code overlay is careful to accommodate a wide range of housing types, with opportunities for single family houses, duplexes, townhomes, and multi-family, provided they incorporate appropriate measures to ensure compatibility with adjacent uses.

### 2.2.5 Business Development: A Place of Lasting Value, New Investment, and Economic Activity





## VISION

*Successful character towns have meaningful local businesses. They provide authentic and experience-based shopping that can enrich and activate the entire community; however, this isn't easy. The small business environment in any community is challenging, particularly in today's world of changing retail. The leasable retail space in downtowns can be challenging; often defined by aged, irregular building stock that while charming in character, may not be ideal in function. Having a meaningful and sustainable retailing experience as part of the downtown experience requires concentrated effort and collaboration between the public and private sector.*

### Big Move: Business Supportive City Government

The First Step in facilitating successful locally owned business is for local government to embrace their value and support their needs. Small Business Owners are typically working against the clock, with very significant personal borrowing, small initial profit margins and long hours of work to get through difficult startup periods. Timely, concierge service for issues such as permits, occupancy, marketing, advertising and events collaboration tell the small business community that they are valued and that local government is their partner. Local Governments that do not embrace this philosophy generally do not have vibrant locally owned, historic, 'Main Street' business districts.

### Big Move: Programs to Support New Small Business

Many historic downtowns are experiencing reinvestment at a faster pace because the local government and business owners

are working together. This means service oriented building and permitting departments who help business owners find inexpensive, actionable solutions to their needs in a prompt manner. It often means small business incentives in the forms of grants or credits to assist with new signage, façades and shared parking. Marketing programs such as 'Buy Local' can galvanize a community to its local businesses, enhancing the sense of place and social connection. Events collaboration between local government and various community groups are targeted to bring people and purchasing activity to the local businesses. This is accomplished by aggressively increasing the 'virtual presence' of downtown marketing & advertising as well as the 'physical presence' of regular foot traffic to the community at large.

### Big Move: Modernize Space + Upgrade Historic Buildings

Historic downtowns have historic buildings which are cherished by the community for their character,

but sometimes cursed by their owners and tenants for their problems. Sometimes, historic buildings do not meet local fire or access codes, have inadequate space for modern retailing, and may have outdated electrical, plumbing, fiber and other building systems. The complexities and costs in addressing these problems can become adversarial between building owner, tenant and local government. Progressive communities have developed specialized programs to assist in facilitating the renovation of space as well as adopting unique standards to allow for local interpretation of retrofit requirements.

### Big Move: Strong Neighborhoods

The strength of any downtown is directly tied to the strength of its neighborhoods. Investments in stormwater infrastructure, streetscapes, public buildings, or parks cannot make downtown a great place if the neighborhoods are in decline. A major goal of this plan is to provide a framework within which neighborhoods can strengthen and remain strong over time. Besides the basic infrastructure improvements described above, strengthening neighborhoods is accomplished through a form-based code approach to new development that promotes compatibility.

### Big Move: New Quality Development

Downtown has been passed over by new quality development since the 1990's for several reasons described throughout this report, including: poor access and visibility, less than ideal public image, lack of sewer and stormwater infrastructure, a block and parcel structure that doesn't fit with the current development code, etc. The foundational purpose of this Master Plan is to provide the City with a roadmap for improving or rectifying as many of those situations as possible, in order to attract new quality development in downtown, and improve the lives of Ocoee residents and business owners.

### Big Move: Coordinated Main Street + Park Activities + Social Media

During the public input process, some residents and business owners expressed concern that there was not enough coordination or communication between the various public and private entities that conduct events downtown. Though not directly addressed in this Master Plan, the City will review its current process for promoting downtown events, with a focus on linking other activities available. It will also review its use of event promotion on social media for ways of reaching a broader audience.



## 2.3 MASTER PLAN IMPLEMENTATION

A list of projects has been identified by the City for implementing components of the Long Term Master Plan Vision described in this chapter. The prioritized list below contains the projects and their associated cost estimates, which are labeled on the map. The projects, descriptions, and rough-order-magnitude (ROM) costs are conceptual in nature and require additional levels of design and consideration as the implementation phase moves forward. Many factors will affect the actual implementation of the Long Term Master Plan Vision, including land assembly, ROW acquisition, construction costs, private development, and others.



# PROJECT LIST

Map #	Project Name	Project Description	Cost Estimate
1	Master Downtown Stormwater System	Retention Pond & Collection System Trunk Lines; includes Creek Improvements	\$8,180,000
--	Downtown Gravity Sewer System	Build as Needed; Locations TBD by development & other projects	TBD
2	Bluford Avenue Reconstruction	Delaware St to Silver Star Rd; includes Force Main & Underground Power	\$9,680,000
3	Lakefront Park Improvements Phase 1	Lakefront improvements and park development (southern section)	\$2,200,000
4	City Hall Relocation	Construct New Building; includes Parking Facility	\$9,000,000
5	Lakeshore Center Expansion	Add 350-seat Ballroom	\$4,000,000
6	Oakland Avenue Reconstruction	Taylor St to Lakeshore Dr; includes Trail Connector Segment 1	\$895,000
7	McKey Street Reconstruction	Bluford Ave to Lakewood Ave	\$300,000
--	Wastewater Force Main Connector	North Segment, Silver Star Rd to Wurst Rd	\$2,875,000
--	Maine Street Extension	Maguire Rd to Bluford Ave	\$750,000
8	Ocoee Woman's Club Rehabilitation	Rebuild kitchen, adjust grading, and make other improvements (Long-term Lease)	\$200,000
--	Bluford Second Left-turn Lane	North Approach to SR 50	\$350,000
--	Silver Star Road Realignment - PD&E	Project Development & Environmental Study (by FDOT)	\$890,000
9	Kissimmee Avenue Realignment	Floral St to McKey St; Floral St RR crossing upgrade	\$261,000
10	Taylor Street Reconstruction	McKey St to Franklin St with Roundabouts; includes Trail Connector Segment 2	\$956,000
11	Gateways & Wayfinding Phase 1	Franklin@Taylor, McKey@Taylor, Geneva@Bluford	\$80,000
12	Trail Connector Segment 3	East Crown Point Rd to Taylor St (south side of Franklin, cross Plant St at E Crown Pt Rd)	\$375,000
13	Lakefront Park Improvements Phase 2	Lakefront improvements and park development (northern section)	\$2,520,000

**FUNDED PROJECTS**

**Funded Projects Subtotal \$43,512,000**

Map #	Project Name	Project Description	Total Cost
14	Franklin Street Improvements	Ocoee-Apopka Rd to Kissimmee Ave	\$1,100,000
15	Oakland Avenue Extension Phase 1	Maguire Rd to Taylor Rd	\$1,800,000
16	McKey Street Extension Phase 1	Maguire Rd to Taylor Rd	\$1,650,000
17	Kissimmee Avenue Construction	Constructs New Street from McKey St to Silver Star Rd	\$1,635,000
18	McKey Street Refresh	Modify Taylor St to Bluford Ave segment to match new standard	\$135,000
19	McKey Street Extension Phase 2	Roper/Enterprise St to Maguire Rd	\$2,955,700
20	Silver Star Road Realignment (FDOT)	Connect to Oakland Ave Ext Phase 2; includes Bluford and Oakland Roundabouts	TBD
21	Oakland Avenue Extension Phase 2	Silver Star Rd/Franklin St to Maguire Rd	\$1,200,000
22	Gateways & Wayfinding Phase 2	Silver Star@Oakland, Silver Star@Bluford	\$60,000
23	Trail Connector via Palm Dr	West Crown Point Rd to Franklin St (Second Pathway)	\$1,000,000
24	Taylor Street Realignment	McKey St to Silver Star Rd; Creates Rectangular Grid Street Pattern	\$1,350,000
--	McGuire Rd/Kissimmee Ave Intersection	Reconstruct Maguire Rd Intersection	\$1,200,000
--	Maguire Road Widening	Maine St to Orlando Ave/Kissimmee Ave; includes Maguire Rd Phase 5	\$2,100,000
25	Kissimmee Avenue Reconstruction	Maguire Rd/Orlando Ave to Floral St; includes RR crossing upgrade	\$330,000
--	Bluford Ave/Geneva St Roundabout	Construct a roundabout at this intersection; replaces traffic signal	\$350,000
--	Code Modernization	Revise Land Development Code	\$500,000

**UNFUNDED PROJECTS**

**Unfunded Projects Subtotal \$17,365,700**

**Downtown Projects Total \$60,877,700**





## DOWNTOWN CORE

Big things are coming to Ocoee. The journey will be longer and harder than anyone expects, but the results will be worth it if you can stay the course. In the end, the downtown will have the infrastructure it needs to grow, the vision to keep it authentic, and people to make it vibrant.

The birds-eye perspective on the left shows a future vision of the downtown core, complete with the proposed infrastructure and park projects with infill redevelopment scattered throughout.

The perspective is centered above the intersection of McKey Street and Taylor Street, looking roughly northeast towards Starke Lake. The relevant projects from the recommended list are labeled.

- 1 Master Downtown Stormwater System
- 2 Bluford Avenue Reconstruction
- 3 Lakefront Park Improvements Phase 1
- 4 City Hall Relocation
- 5 Lakeshore Center Expansion
- 6 Oakland Avenue Reconstruction
- 7 McKey Street Reconstruction
- 8 Ocoee Woman's Club Rehabilitation
- 13 Lakefront Park Improvements Phase 2
- 18 McKey Street Refresh



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5:30am-2:00pm  
FRI 5:30am 8:00pm  
SUN. 7:00am-2:00pm

*Dinner*  
Every Friday Till 8 pm



3

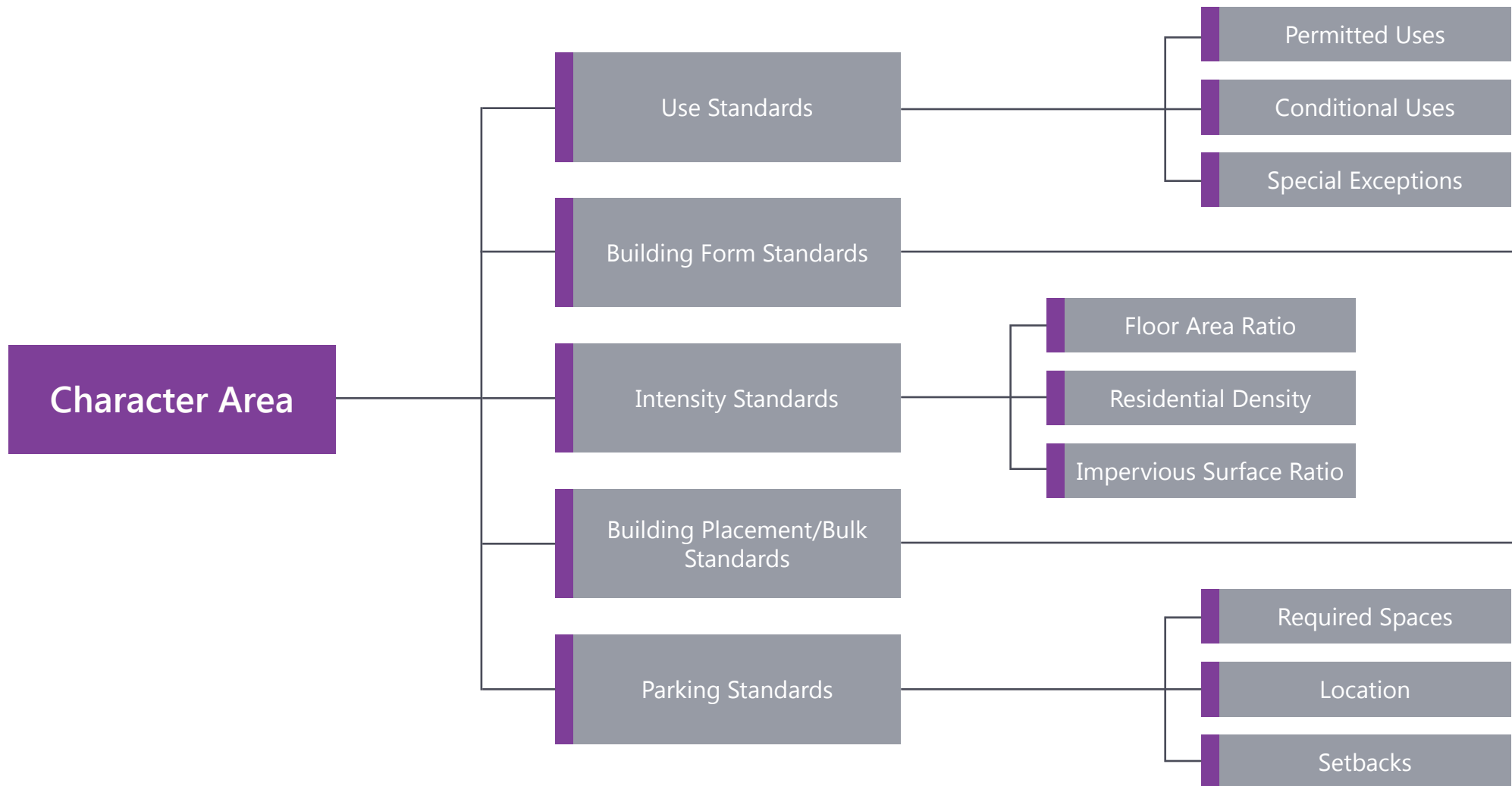
Downtown Code Overlay

FIGURE 3.2 - CODE ORGANIZATION

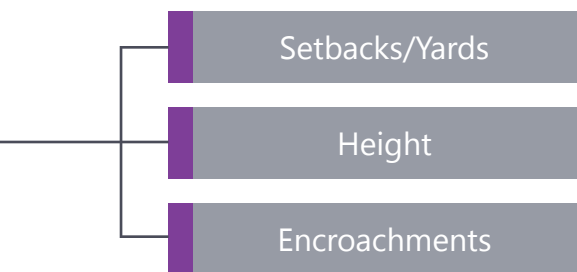
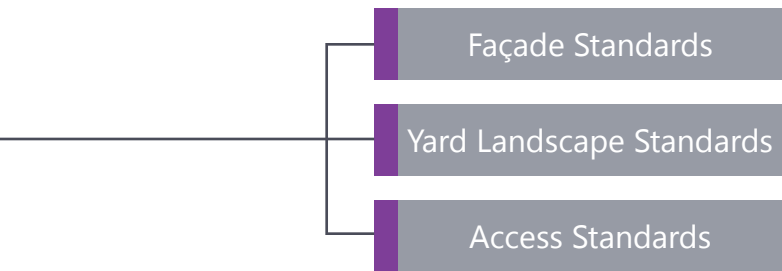
CHARACTER IDENTIFIER

REGULATORY CLASS

DEVELOPMENT STANDARDS







## 3.0 DOWNTOWN CODE OVERLAY

### 3.1 Purpose and Intent

The purpose of the Downtown Code Overlay and the standards contained within is to provide a regulatory framework tailored specifically to the downtown Ocoee context and the desired future character described by residents, business owners, and City officials. The goal of the overlay is not to add additional burden to development, but to provide a regulatory environment that is more likely to promote the desired form and character of downtown Ocoee.

### 3.2 Components

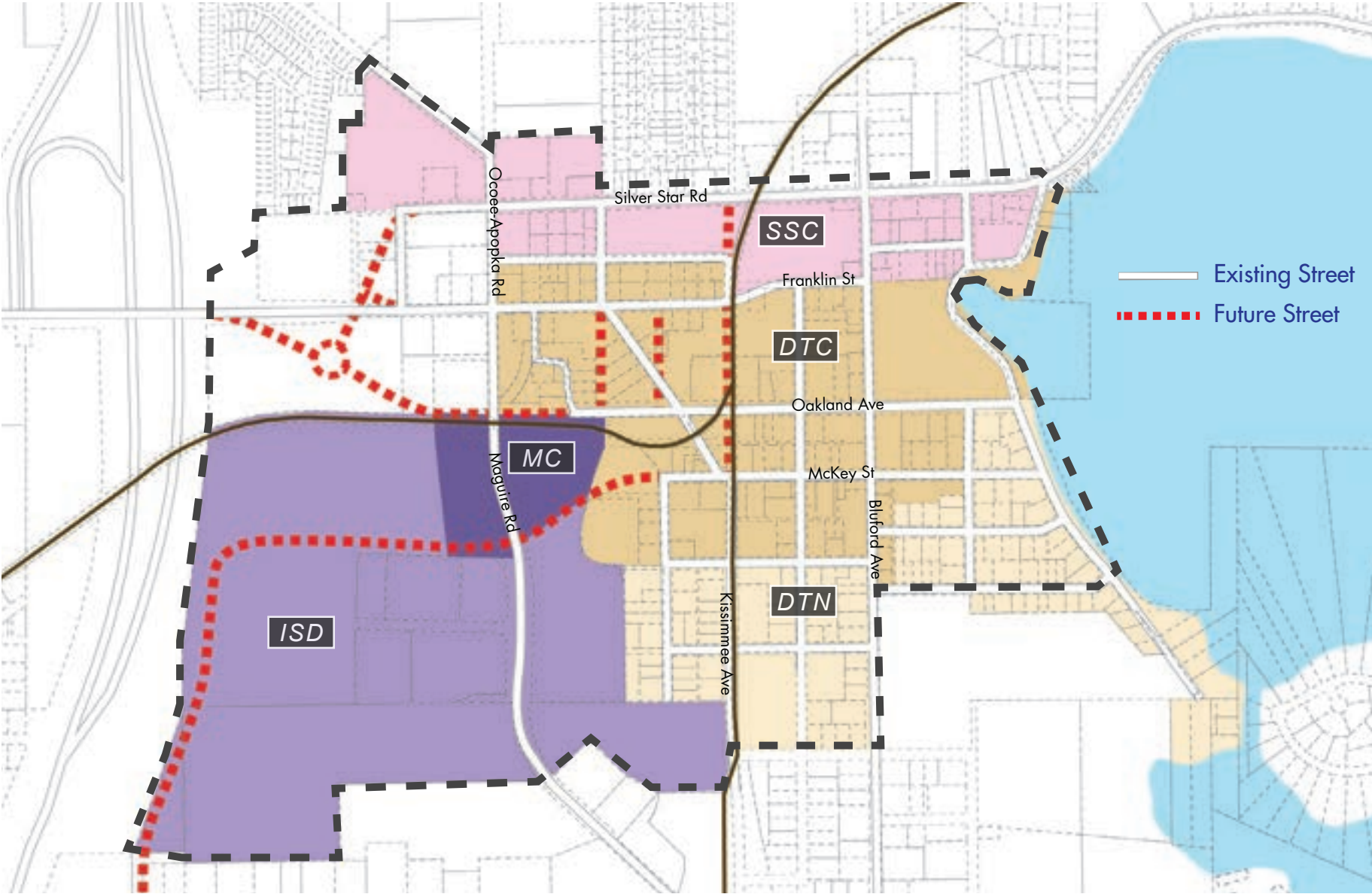
The Downtown Code Overlay (Overlay) is established as a type of form-based code that focuses on the relationships of buildings to their surrounding public and private contexts, not simply by what zoning classification they fall under. The foundation of the form-based code is built on Character Areas and Road Hierarchy. All standards that apply to development stem from the combination of these two fundamentals, establish the context, and reflect the desired uses and building forms specific to that context. The relationship of the components of the form-based code is generally shown in the graphic to the left.

### 3.3 Applicability

The Downtown Code Overlay will apply to all parcels within the boundaries of a regulating Character Area, as described in Section 3.4, and will supersede any development standards currently in place. If the Overlay is silent to a particular standard or regulation, the underlying City of Ocoee Land Development Code shall prevail. In the case of new development within a Character Area, the Overlay will automatically apply. In the case of existing development within a Character Area being altered or improved, the Overlay will apply if any of the following conditions are met:

- Within any 5-year period following the enactment of the Overlay, the floor area of the building has increased by more than 30%;
- Within any 5-year period following the enactment of the Overlay, more than 50% of a building has been replaced;
- Within any 5-year period following the enactment of the Overlay, total cost of improvements exceeds 50% of the assessed value of the building.

FIGURE 3.4 - CHARACTER AREAS MAP



### 3.4 Character Areas

The Master Plan establishes six different “Character Areas” for the purpose of defining the desired future character of various portions of the downtown study area. For each of these six categories the City has different goals for the types of uses, the intensity of the uses, and the overall urban form for development. The specific locations for the Character Areas are shown in the map to the left and the general characteristics and vision for each are outlined below. Section 3 of this report includes design standards intended to implement the established vision.

#### 3.4.1 Silver Star Corridor (SSC)

- Frames the northern edge of downtown
- Gateway into downtown from the east and links to 429 Interchange
- Less dense/intense than the Franklin character area, but greater than Downtown Core
- Mix of residential and commercial
- Less urban in form than Franklin or Downtown Core character areas
- Less formal with varying setbacks

#### 3.4.2 Downtown Core (DTC)

- Encompasses historic downtown Ocoee, lakefront, and immediate surrounding area
- Streets shall maintain and enhance historic block structure and character
- Takes advantage of access and proximity to lakefront
- Urban setbacks and design
- Pedestrian and bicycle oriented
- Mix of residential and commercial infill and redevelopment
- Creates more opportunities for people to come downtown and stay for the experience

#### 3.4.3 Downtown Neighborhood (DTN)

- Existing residential areas surrounding the Downtown Core
- Maintain historic block structure
- Should remain fundamentally low density in form
- Some non-residential uses may be allowed along Bluford but they must adhere to the “House” building type and standards

#### 3.4.4 Maguire Corridor (MC)

- Transitional area between industrial and commercial south of the railroad tracks, along Maguire Road
- Currently in an industrial zone, and depending on market conditions, may build out as industrial
- If McKey Street is extended to the west from downtown, this area could function more as a convenience commercial area
- Both industrial and light commercial uses and building types are allowed in this district

#### 3.4.5 Industrial Special District (ISD)

- Vacant land controlled by single owner
- Focused on larger scale employment uses such as office, light industrial, and warehouse
- To be developed in a cohesive manner
- High visibility from SR 429

- No outdoor storage or manufacturing activities
- Landscaped buffers from along Maguire Road and SR 429
- Opportunity to create an additional connection to the industrial park to the south via a further extension of McKey Street





### 3.6 Urban Standards

The urban standards in this section describe the required urban design characteristics of new development within the Overlay including: density/intensity, building placement, building bulk standards, parking, encroachments, and more. The applicability of these standards to a specific property is described in Section 3.3 and is dependent on the character area of the individual property.

The urban standards for the overlay are given in Figures 3.6(a) and 3.6(b). Diagrams describing how various standards are applied are provided in Figures 3.6.1, 3.6.2(a), and 3.6.2(b) Note that additional standards pertaining to lots, buildings, and landscaping are found in the Building Types and Facade Types sections.

#### 3.6.1 Building Placement

The placement of buildings on a lot is dictated by setbacks/yard requirements and frontage requirements. These requirements are affected by the character area the lot is located in and the adjacency of streets or other lots. A reference diagram of building placement standards is shown in Figure 3.6.1. The standards themselves are provided in Figures 3.6(a) and 3.6(b).

##### Street Setbacks

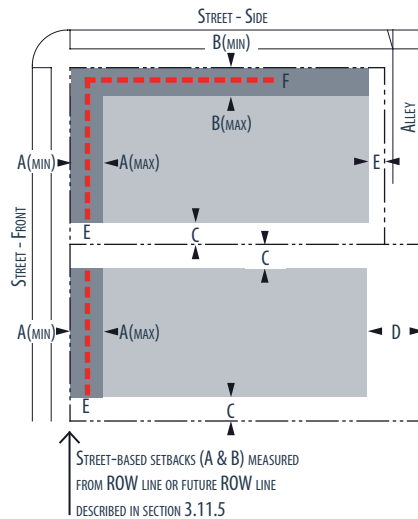
To ensure that the future urban character of downtown Ocoee

is achieved, both minimum and maximum setbacks are provided for every street, and selected future streets in Figure 3.6(b). All setbacks must be landscaped according to the requirements of each Building Type, as specified in Section 3.7.

##### Frontages

A frontage is defined as the percentage of a lot's dimension adjacent to a street that must have a building's ground floor within the street setback area. Building frontages must have ground floor facade types that are approved based on its building type. Approved Facades are described in Section 3.8. Building Types are described in Section 3.7.

FIGURE 3.6.1 - BUILDING PLACEMENT DIAGRAM



#### 3.6.2 Parking Placement

Off-street parking facilities are permitted only in certain zones within a lot, depending on the character area the lot is located in. Furthermore, parking lot setbacks are required within these zones. Reference diagrams for Parking Zone locations and setbacks are provided in Figures 3.2.6(a) and (b) and the standards for each character area are shown in Figure 3.6.4.

#### 3.6.3 Drive-Throughs

Drive-Throughs are not permitted within the Overlay.

#### 3.6.4 Encroachments

Encroachments into the ROW such as awnings, marquees, balconies, arcades, projecting wall signs, and out-door dining may be permitted according to the character area, as shown in Figure 3.6(a) and described in Section 3.9.

FIGURE 3.6.2(A) - PARKING ZONES

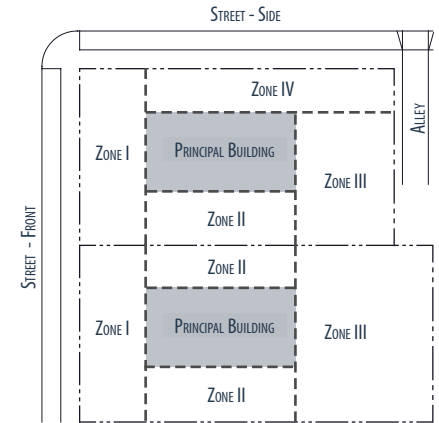


FIGURE 3.6.2(B) - PARKING SETBACKS

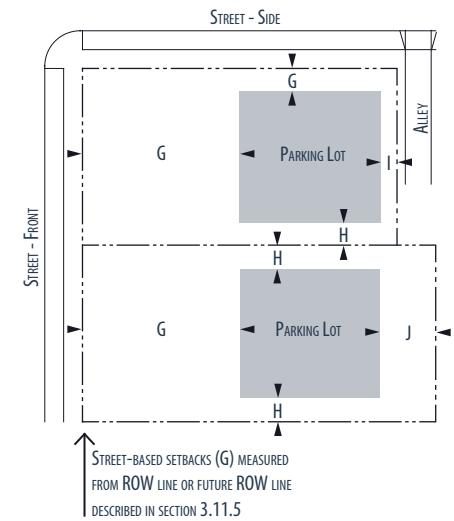


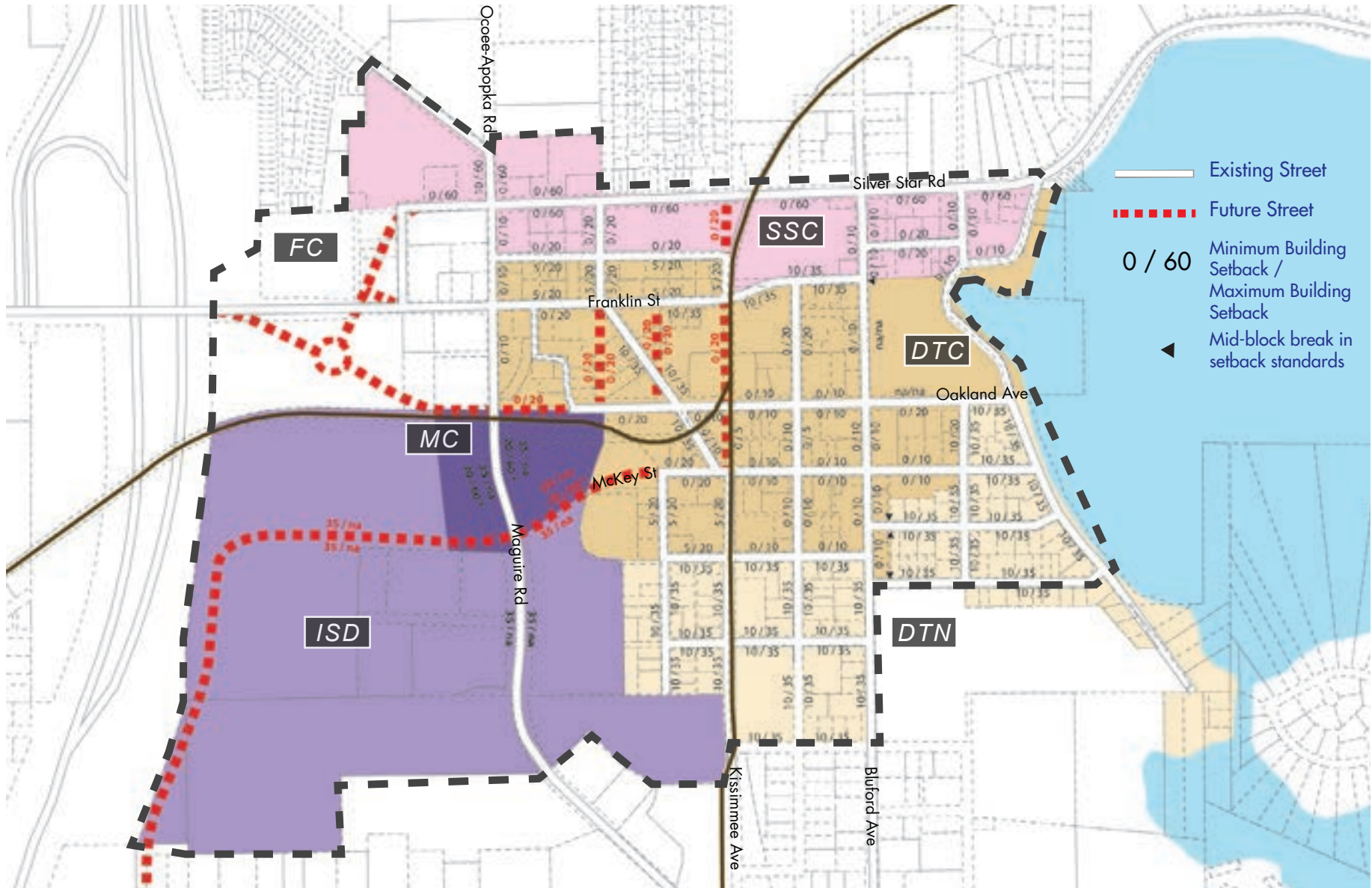
FIGURE 3.6(A) - TABLE OF URBAN STANDARDS

	Downtown Core	Silver Star Corridor	Maguire Corridor	Downtown Neighborhood	Industrial Special District
<b>Density/Intensity Standards</b>					
Max Block Perimeter	2000'	2500'	2000'	1800'	4600'
Max Gross Residential Density	30 du/acre	30 du/acre	n/a	12 du/acre	n/a
Max Gross Intensity (FAR)	1.0	1.5	0.5 / 0.7 <sup>6</sup>	n/a	0.7
Max Impervious Surface	0.85	0.7	0.7 / 0.85 <sup>6</sup>	0.5	0.85
<b>Building Placement/Bulk Standards<sup>3</sup></b>					
A(min) - Min Street Setback <sup>4</sup>	see Figure 3.6(b)				
A(max) - Max Street Setback <sup>4</sup>	see Figure 3.6(b)				
B - Min Side Yard Setback	0'	10'	10'	6'	10'
C - Min Rear Yard Setback	15'	20'	10'	15'	10'
D - Min Alley Setback	3'	3'	3'	3'	3'
E - Min Building Frontage on Front Street	60%	50%	30%	30%	na
F - Min Building Frontage on Side Street	30%	30%	0%	30%	na
Max Stories	3	3	1	3	6
Max Height	40'	60'	40'	35'	75'
Max Height First Floor <sup>5</sup>	14'	14'	14'	14'	n/a
<b>Off-Street Parking Standards</b>					
Permitted Auto Parking Zones	II, III	I, II, III, IV	I, II, III, IV	II, III, IV	I, II, III, IV Truck Bays - II, III
G - Min Setback from Street	same as max bld	5'	10'	same as max bld	25'
H - Min Side Setback	5'	5'	5'	5'	5'
I - Min Rear Setback	5'	5'	5'	5'	5'
J - Min Alley Setback	3'	3'	3'	3'	3'
<b>Encroachments</b>					
Encroachments Allowed by Permit	Awnings, Marquees, Balconies, Arcades, Outdoor Dining, Projecting Wall Sign	Awnings, Balconies, Outdoor Dining, Projecting Wall Sign	Awnings	N/A	Awnings

**Notes**

- 1 Residential allowed only if part of a vertically mixed use building.
- 2 1.5 F.A.R. if single use, 3.0 F.A.R. if including mixed use residential.
- 3 Minimum principal building setback from any natural surface water body or retained wetland is 50 ft from the normal high water.
- 4 Street setbacks based on future ROW lines per street cross-sections in section 3.11.5.
- 5 Floor Heights do not include non-habitable spaces such as towers, parapets, gables, etc.
- 6 Commercial Uses / Industrial Uses

FIGURE 3.6(B) - BUILD-TO ZONE REQUIREMENTS





### 3.7 Building Types

There are eight building types permitted within the Overlay, each in accordance with the applicable character area, as shown in Figure 3.7. Building Types permitted within the Overlay include:

- Commercial Building
- Duplex-Quadplex
- Townhouse
- Tuck-Under Townhome
- Double Loaded Apartment
- House
- Civic/Institutional
- Industrial Building

Each building type has a schematic example, illustrative photos, regulations, and standards that are required for development within the overlay. There are six categories of regulations discussed for each type, which are described as follows.

**Lot Requirements:** Provisions for minimum and maximums; lot depth, lot size and unit size (where applicable).

**Access Standards:** Provisions for location of the pedestrian access (e.g. building entrances) and vehicular access (e.g. garages and parking). Utility service access is also addressed.

**Facade Standards:** Provisions for a variety of different street/public frontage types that are permitted by building type.

**Landscape Standards:** All yards shall be landscaped consistent with the standards required according to building type.

FIGURE 3.7 - TABLE OF PERMITTED BUILDING TYPES

Building Type	Downtown Core	Silver Star Corridor	Maguire Corridor	Downtown Neighborhood	Industrial Special District
Commercial Building	✓	✓	✓		
Apartment Building	✓	✓			
Townhome	✓	✓		✓	
Tuck-Under Townhome	✓	✓			
Duplex-Quadplex	✓			✓	
House	✓			✓	
Civic/Institutional	✓	✓			
Industrial			✓		✓

### 3.7.1 Building Type: Commercial Building



#### A. General Standards

**Description** - A building configured for retail, service, and/or office uses on the ground floor and upper floors configured for office or residential uses.

**Lot Standards** - Lots containing Commercial buildings shall meet the following standards.

- Minimum lot width: 50'
- Maximum lot width: 200'

**Residential Unit Standards** - For buildings containing residential units above the ground floor:

- Minimum Res. Unit Area: 850 sqft

#### B. Access Standards

**Building Entrance** - The entrance to the non-retail uses within the building shall be through a lobby accessed at street level. Alternatively, ground floor residential units may have direct sidewalk access. All ground floor retail uses shall have direct sidewalk access.

**Interior Circulation** - Interior circulation to individual units shall be through an internal corridor.

**Parking Access** - Parking shall be accessed through an alley, or side (not frontage) street. In mid-block locations without existing alleys, new alleys shall be provided and accessed from the frontage street by a driveway or multiple driveways of not more than 20' in aggregate width.

**Services** - All utility access, above ground equipment, and trash receptacles shall be located at the rear of the lot and accessed by alley or driveway of not more than 10' in width.

#### C. Open Space Standards

**Rear Yards** - Rear yards organized as a courtyard are the primary shared open space and shall be no less than 10% of the area of each lot. This yard may be provided on the ground or on a podium.

**Front Yards** - Front yards are defined by the applicable zone setback and frontage type requirements.

#### D. Facades

**Required Facades** - The building must comply with the following facade requirements:

Primary Facades	% Min	% Max
Commercial Facade	75	100
Porch	--	100
Stoop	--	10

Secondary Facades	% Min	% Max
Commercial Facade	45	100
Porch	--	100
Stoop	--	10

#### F. Landscape Standards

**Description** - All yards shall be landscaped consistent with the following specific standards and the general landscape standards. Walks from adjacent sidewalks to building entrances are allowed in all yards.

**Front Yards** - When a front yard is present, the yard shall contain landscape and hardscape consistent with the identified future design of the adjacent sidewalk.

**Side Yards** - Side yards, where present, shall contain at least 40% coverage by shrubs or ground cover.

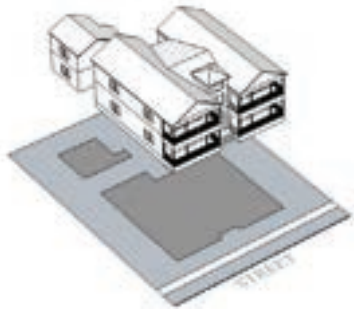
**Rear Yards** - Rear yards shall contain at least one shade tree for every 50 linear feet of rear lot line. Trees may be placed in groups so long as the total number of trees satisfies the requirement. The remainder of the yard shall contain shrubs, ground cover, or turf; at least 40% of the area shall be planted in shrubs and/or ground cover.

#### G. Illustrative Photos

**Description** - The following photos illustrate the design intent of the Commercial building type.



### 3.7.2 Building Type: Apartment



#### A. General Standards

**Description** - A building containing multiple dwellings above and/or beside each other.

**Lot Standards** - Lots containing Apartment buildings shall meet the following standards.  
 Minimum lot width: 75'  
 Maximum lot width: 200'

**Unit Size Standards** - Minimum unit size shall be 850 square feet.

#### B. Access Standards

**Building Entrance** - The entrance to the building shall be through lobby accessed at street level. Alternatively, ground floor units may have direct sidewalk access.

**Interior Circulation** - Interior circulation to individual units shall be through an internal corridor.

**Parking Access** - Garages and parking shall be accessed through an alley, or side (not frontage) street. In mid-block locations without existing alleys, new alleys shall be provided and accessed from the frontage street by a driveway or multiple driveways of not more than 20' in aggregate width.

**Services** - All utility access, above ground equipment, and trash receptacles shall be located at the rear of the lot and accessed by alley or driveway of not more than 10' in width.

#### C. Open Space Standards

**Rear Yards** - Rear yards organized as a courtyard are the primary shared open space and shall be no less than 15% of the area of each lot.

**Front Yards** - Front yards are defined by the applicable zone setback and frontage type requirements.

**Side Yards** - Side yards are required with Apartment building types and shall be at least 5' in width.

#### D. Facade Standards

**Required Facades** - The building must comply with the following facade requirements:

Primary Facades	% Min	% Max
Porch	--	100
Stoop	--	50

**Orientation** - At the ground level, living areas (living room, dining room, etc...) should be oriented toward street frontage to the greatest degree possible; service areas shall be oriented toward rear yard or alley frontages.

#### E. Building Size and Massing Standards

**Massing** - Buildings shall be composed of multiple volumes following the requirements of the applicable zone. In order to provide for a streetscape with

varying building heights, height ratios for Apartment Buildings shall be as shown below.

#### F. Landscape Standards

**Description** - All yards shall be landscaped consistent with the following specific standards. Walks from adjacent sidewalks to building entrances are allowed in all yards.

**Front Yards** - If the front yard is less than 5' deep, the yard shall contain landscape and hardscape consistent with the identified future design of the adjacent sidewalk. Front yards more than 5' deep shall contain at least one understory tree for every 30 linear feet of front lot line. Trees may be placed in groups so long as the total number of trees satisfies the requirement. The remainder of the yard shall contain shrubs or ground cover which may be planted on the ground or in planters less than 36" in height.

**Side Yards** - Side yards shall contain at least one understory tree for every 30 linear feet of side lot line. Trees may be placed in groups so long as the total number of trees satisfies the requirement. The remainder of the yard shall contain shrubs, ground cover, or turf; at least 40% of the area shall be planted in shrubs and/or ground cover.

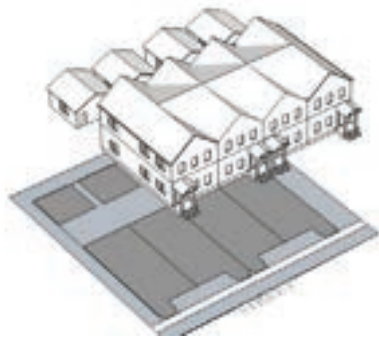
**Rear Yards** - Rear yards shall contain at least one shade tree for every 50 linear feet of rear lot line. Trees may be placed in groups so long as the total number of trees satisfies the requirement. The remainder of the yard shall contain shrubs, ground cover, or turf; at least 40% of the area shall be planted in shrubs and/or ground cover.

#### G. Illustrative Photos

**Description** - The following photos illustrate the design intent of the Apartment building type.



### 3.7.3 Building Type: Townhouse



#### A. General Standards

**Description** - A building containing multiple dwellings beside each other in a commonwall condition.

**Lot Standards** - Lots containing Townhouse buildings shall meet the following standards.  
Minimum lot width: 80'  
Maximum lot width: 150'

**Unit Size Standards** - Minimum unit size shall be 850 square feet. Minimum unit frontage shall be 20'.

#### B. Access Standards

**Building Entrance** - The primary entrance to each residential unit shall be accessed directly from and face the street.

**Parking Access** - Garages and parking shall be accessed through an alley, or side (not frontage) street. In mid-block locations without existing alleys, new alleys shall be provided and accessed from the frontage street by a driveway or multiple driveways of not more than 20' in aggregate width.

**Services** - All utility access, above ground equipment, and trash receptacles shall be located at the rear of the lot and accessed by alley or driveway of not more than 10' in width.

#### C. Open Space Standards

**Rear Yards** - Rear yards shall be no less than 15% of the area of each lot. Yard space between detached garages and the primary unit counts toward this requirement.

**Front Yards** - Front yards are defined by the applicable zone setback and frontage type requirements.

#### D. Facade Standards

**Required Facades** - The building must comply with the following facade requirements:

Primary Facades	% Min	% Max
Porch	--	100
Stoop	--	50

**Orientation** - Service areas shall be oriented toward rear yard or alley frontages.

#### E. Building Size and Massing Standards

**Massing** - Buildings shall be composed of 2 and/or 3 story volumes following the requirements of the applicable zone.

**Corner Lots** - Building on corner lots shall be designed with a front facade on each street frontage.

#### F. Landscape Standards

**Description** - All yards shall be landscaped consistent with the following specific

standards. Walks from adjacent sidewalks to building entrances are allowed in all yards.

**Front Yards** - If the front yard is less than 5' deep, the yard shall contain landscape and hardscape consistent with the identified future design of the adjacent sidewalk. Front yards more than 5' deep shall contain at least one understory tree for every 30 linear feet of front lot line. Trees may be placed in groups so long as the total number of trees satisfies the requirement. The remainder of the yard shall contain shrubs or ground cover which may be planted on the ground or in planters less than 36" in height.

**Side Yards** - Side yards shall contain at least one understory tree for every 30 linear feet of side lot line. Trees may be placed in groups so long as the total number of trees satisfies the requirement. The remainder of the yard shall contain shrubs, ground cover, or turf; at least 40% of the area shall be planted in shrubs and/or ground cover.

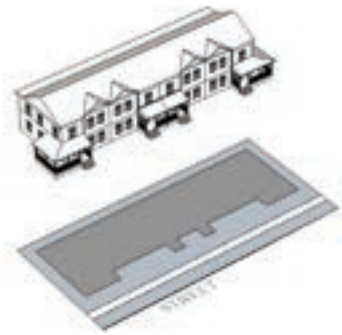
**Rear Yards** - Rear yards shall contain at least one shade tree for every 50 linear feet of rear lot line. Trees may be placed in groups so long as the total number of trees satisfies the requirement. The remainder of the yard shall contain shrubs, ground cover, or turf; at least 40% of the area shall be planted in shrubs and/or ground cover.

#### G. Illustrative Photos

**Description** - The following photos illustrate the design intent of the Townhouse building type.



### 3.7.4 Building Type: Tuck-Under Townhouse



#### A. General Standards

**Description** - A building on a lot with no rear yard containing multiple dwellings beside each other in a commonwall condition where garages are located under the rear of the building and accessed by an alley.

**Lot Standards** - Lots containing House buildings shall meet the following standards.  
 Minimum lot width: 100'  
 Maximum lot width: 200'

**Unit Size Standards** - Minimum unit size shall be 850 square feet. Minimum unit frontage shall be 22'.

#### B. Access Standards

**Building Entrance** - The primary entrance to each residential unit shall be accessed directly from and face the street.

**Parking Access** - Garages and parking shall be accessed through an alley or side (not frontage) street. In mid-block locations without existing alleys, new alleys shall be provided and accessed from the frontage street by a driveway or multiple driveways of not more than 20' in aggregate width.

**Services** - All utility access, above ground equipment, and trash receptacles shall be located at the rear of the lot and accessed

by alley or driveway of not more than 10' in width.

#### C. Open Space Standards

**Front Yards** - Front yards are defined by the applicable zone setback and frontage type requirements.

**Private Patios** - Each dwelling shall have at least 200sf of private patio space.

#### D. Facade Standards

**Required Facades** - The building must comply with the following facade requirements:

Primary Facades	% Min	% Max
Porch	--	100
Stoop	--	50

**Orientation** - Service areas shall be oriented toward rear yard or alley frontages.

#### E. Building Size and Massing Standards

**Massing** - Buildings shall be composed of 2 and/or 3 story volumes following the requirements of the applicable zone.

**Corner Lots** - Building on corner lots shall be designed with a front facade on each street frontage.

#### F. Landscape Standards

**Description** - All yards shall be landscaped consistent with the following specific standards. Walks from adjacent sidewalks to building entrances are allowed in all yards.

**Front Yards** - If the front yard is less than 5' deep, the yard shall contain landscape and hardscape consistent with the identified future design of the adjacent sidewalk. Front yards more than 5' deep shall contain

at least one understory tree for every 30 linear feet of front lot line. Trees may be placed in groups so long as the total number of trees satisfies the requirement. The remainder of the yard shall contain shrubs or ground cover which may be planted on the ground or in planters less than 36" in height.

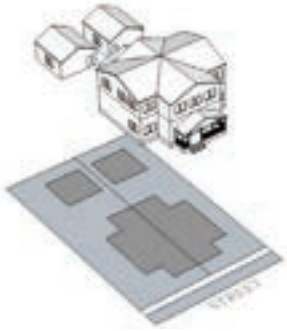
**Side Yards** - Side yards shall contain at least one understory tree for every 30 linear feet of side lot line. Trees may be placed in groups so long as the total number of trees satisfies the requirement. The remainder of the yard shall contain shrubs, ground cover, or turf; at least 40% of the area shall be planted in shrubs and/or ground cover.

#### G. Illustrative Photos

**Description** - The following photos illustrate the design intent of the Tuck-Under Townhouse building type.



### 3.7.5 Building Type: Duplex-Quadplex



#### A. General Standards

**Description** - A building containing two, three, or four dwellings above and/or beside each other in a building that is similar in form to a large single-family house.

**Lot Standards** - Lots containing Apartment buildings shall meet the following standards.  
Minimum lot width: 50'  
Maximum lot width: 100'

**Unit Size Standards** - Minimum unit size shall be 850 square feet.

#### B. Access Standards

**Building Entrance** - The primary entrance to each residential unit shall be accessed directly from and face the street.

**Parking Access** - Garages and parking shall be accessed through an alley or side (not frontage) street. In mid-block locations without existing alleys, new alleys shall be provided and accessed from the frontage street by a driveway or multiple driveways of not more than 20' in aggregate width.

**Services** - All utility access, above ground equipment, and trash receptacles shall be

located at the rear of the lot and accessed by alley or driveway of not more than 10' in width.

#### C. Open Space Standards

**Rear Yards** - Rear yards shall be no less than 15% of the area of each lot.

**Front Yards** - Front yards are defined by the applicable zone setback and frontage type requirements.

**Porches, Stoops, and Patios** - Porches and stoops may encroach upon any required setbacks to the extent allowed by zone standards. Private patios are allowed to encroach only on side or rear setbacks to the extent allowed by zone standards.

#### D. Facade Standards

**Required Frontages** -The building must comply with the following facade requirements:

Primary Facades	% Min	% Max
Porch	--	100
Stoop	--	50

**Orientation** - Service areas shall be oriented toward rear yard or alley frontages.

#### E. Building Size and Massing Standards

**Massing** - Buildings shall be composed of multiple volumes following the requirements of the applicable zone. In order to provide for a streetscape with varying building heights, height ratios for Duplex-Quadraplex buildings shall be as shown below. Half stories are allowed within the roof form.

**Corner Lots** - Building on corner lots shall be designed with a front facade on each street frontage.

#### F. Landscape Standards

**Description** - All yards shall be landscaped consistent with the following specific standards. Walks from adjacent sidewalks to building entrances are allowed in all yards.

**Front Yards** - If the front yard is less than 5' deep, the yard shall contain landscape and hardscape consistent with the identified future design of the adjacent sidewalk. Front yards more than 5' deep shall contain at least one understory tree for every 30 linear feet of front lot line. Trees may be placed in groups so long as the total number of trees satisfies the requirement. The remainder of the yard shall contain shrubs or ground cover which may be planted on the ground or in planters less than 36" in height.

**Side Yards** - Side yards shall contain at least one understory tree for every 30 linear feet of side lot line. Trees may be placed in groups so long as the total number of trees satisfies the requirement. The remainder of the yard shall contain shrubs, ground cover, or turf; at least 40% of the area shall be planted in shrubs and/or ground cover.

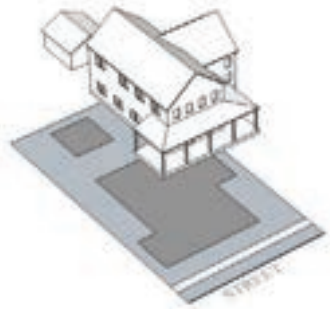
**Rear Yards** - Rear yards shall contain at least one shade tree for every 50 linear feet of rear lot line. Trees may be placed in groups so long as the total number of trees satisfies the requirement. The remainder of the yard shall contain shrubs, ground cover, or turf; at least 40% of the area shall be planted in shrubs and/or ground cover.

#### G. Illustrative Photos

**Description** - The following photos illustrate the design intent of the Duplex-Quadplex building type.



### 3.7.6 Building Type: House



#### A. General Standards

**Description** - A building containing one primary residence (may also accommodate commercial uses)

**Lot Standards** - Lots containing House buildings shall meet the following standards.  
 Minimum lot width: 40'  
 Maximum lot width: 75'

#### B. Access Standards

**Building Entrance** - Each residential unit shall be accessed directly from the street.

**Parking Access** - Garages and parking shall be accessed through an alley or side (not frontage) street. In mid-block locations without existing alleys, access shall be from the frontage street by a driveway of not more than 12' in width.

**Services** - All utility access, above ground equipment, and trash receptacles shall be located at the rear of the lot and accessed by alley or driveway of not more than 10' in width.

#### C. Open Space Standards

**Rear Yards** - Rear yards shall be no less than 15% of the area of each lot.

**Front Yards** - Front yards shall be no less than 10' deep and are defined by the applicable zone setback and frontage type requirements.

#### D. Facade Standards

**Required Facades** - The building must comply with the following facade requirements:

Primary Facades	% Min	% Max
Porch	--	100
Stoop	--	25

**Orientation** - Service areas shall be oriented toward rear yard or alley frontages.

#### E. Building Size and Massing Standards

**Massing** - Buildings shall be composed of 1 and/or 2 story volumes following the requirements of the applicable zone. Half stories are permitted within the roof form.

#### F. Landscape Standards

**Description** - All yards shall be landscaped consistent with the following specific standards. Walks from adjacent sidewalks to building entrances are allowed in all yards.

**Front Yards** - Front yards shall contain at least one understory tree for every 25 linear feet of front lot line. Trees may be placed in groups so long as the total number of trees satisfies the requirement. The remainder of the yard shall contain shrubs or ground cover which may be planted on the ground or in planters less than 36" in height.

**Side Yards** - Side yards shall contain at least one understory tree for every 30 linear feet of side lot line. Trees may be placed in groups so long as the total number of trees satisfies the requirement. The remainder of the yard shall contain shrubs, ground cover, or turf; at least 40% of the area shall be planted in shrubs and/or ground cover.

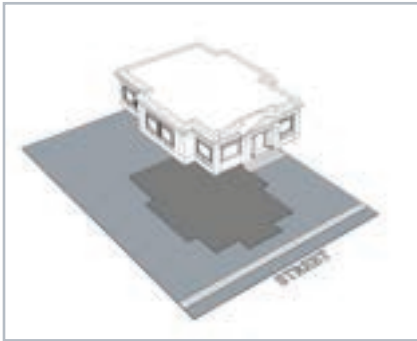
**Rear Yards** - Rear yards shall contain at least one shade tree for every 50 linear feet of rear lot line. Trees may be placed in groups so long as the total number of trees satisfies the requirement. The remainder of the yard shall contain shrubs, ground cover, or turf; at least 40% of the area shall be planted in shrubs and/or ground cover.

#### G. Illustrative Photos

**Description** - The following photos illustrate the design intent of the House building type.



### 3.7.7 Building Type: Civic/ Institutional Building



#### A. General Standards

**Description** - A building designed and located to accommodate public and quasi-public institutional and civic uses (e.g., governmental buildings, schools, hospitals).

**Lot Standards** - Lots containing Civic/ Institutional buildings shall meet the following standards:

- Minimum lot width: 50'
- Maximum lot width: n/a

#### B. Access Standards

**Building Entrance** - The entrance to the building shall have direct sidewalk access.

**Parking Access** - Garages and parking shall be accessed through an alley, or side (not frontage) street. In mid-block locations without existing alleys or driveways, new alleys shall be provided and accessed from the frontage street by a driveway or multiple driveways.

**Services** - All utility access, above ground equipment, and trash receptacles shall be located at the rear of the lot and accessed by alley or driveway.

#### C. Open Space Standards

**Rear Yards** - Rear yards shall be no less than 10% of the area of each lot.

**Front Yards** - Front yards are defined by the applicable zone setback and frontage type requirements.

#### D. Facade Standards

**Required Facades** - To the extent possible, the building must comply the applicable facade requirements. Due to the specialized nature of these uses, facade standards may be modified during project approval phases through the special exception process.

All Street Facades	% Min	% Max
Civic/Inst Facade	65	100
Porch	--	100
Stoop	--	10

**Orientation** - At the ground level, public areas (display, seating, etc...) shall be oriented toward street frontage to the degree possible; service areas shall be oriented toward corridors or the rear of buildings.

#### E. Building Size and Massing Standards

**Massing** - Buildings shall be composed of multiple volumes following the requirements of the applicable zone.

#### F. Landscape Standards

**Description** - All yards shall be landscaped consistent with the following specific standards. Walks from adjacent sidewalks to building entrances are allowed in all yards.

**Front Yards** - If the front yard is less than 5' deep, the yard shall contain landscape and hardscape consistent with the identified future design of the adjacent sidewalk. Front yards more than 5' deep shall contain at least one understory tree for every 30 linear feet of front lot line. Trees may be placed in groups so long as the total number of trees satisfies the requirement. The remainder of the yard shall contain shrubs or ground cover which may be planted on the ground or in planters less than 36" in height.

**Side Yards** - Side yards shall contain at least one understory tree for every 30 linear feet of side lot line. Trees may be placed in groups so long as the total number of trees satisfies the requirement. The remainder of the yard shall contain shrubs, ground cover, or turf; at least 40% of the area shall be planted in shrubs and/or ground cover.

**Rear Yards** - Rear yards shall contain at least one shade tree for every 50 linear feet of rear lot line. Trees may be placed in groups so long as the total number of trees satisfies the requirement. The remainder of the yard shall contain shrubs, ground cover, or turf; at least 40% of the area shall be planted in shrubs and/or ground cover.

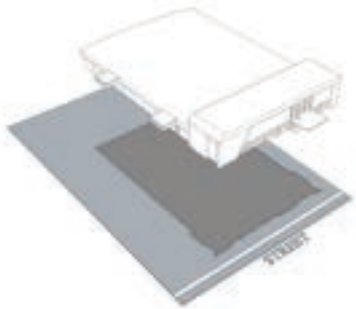
#### G. Illustrative Photos

**Description** - The following photos illustrate the design intent of the Civic/ Institutional building type.





### 3.7.8 Building Type: Industrial Building



#### A. General Standards

**Description** - A building that houses activities related to the manufacture, storage, or distribution of goods.

**Lot Standards** - Lots containing industrial buildings shall meet the following standards.  
 Minimum lot width: 125'  
 Maximum lot width: n/a

#### B. Access Standards

**Building Entrance** - At least one pedestrian entry shall be oriented to the primary frontage, though the building may have several entrances.

**Parking Access** - Vehicle parking areas may be accessed from the frontage street. Where practicable, truck access should be separated from vehicular access via a secondary driveway, though this is not always possible.

**Services** - All utility access, above ground equipment, and trash receptacles shall be located at the rear of the lot and accessed by alley or driveway.

#### C. Open Space Standards

**Rear/Side Yards** - Rear or side yards shall be no less than 10% of the area of each lot.

**Front Yards** - Front yards are defined by the applicable zone setback and frontage type requirements.

#### D. Frontage Standards

**Required Frontages** - The building must comply the applicable zone frontage requirements.

All Street Facades	% Min	% Max
Industrial Facade	65	100

**Orientation** - At the ground level, office spaces should be oriented toward street frontage to the degree possible; service areas shall be oriented toward rear yard or alley frontages.

#### E. Building Size and Massing Standards

**Massing** - Buildings shall be composed of multiple volumes following the requirements of the applicable zone.

#### F. Landscape Standards

**Description** - All yards shall be landscaped consistent with the following specific standards. Walks from adjacent sidewalks to building entrances are allowed in all yards.

**Front Yards** - Front yards shall contain at least three shade trees per 100 feet of frontage. In addition, the front yard shall contain a continuous hedge. At least 40% of the front yard shall contain native shrubs and ground cover. The remainder of the yard may be turf.

**Side Yards** - Side yards, where present, shall contain at least 40% coverage by native shrubs and ground cover. The remainder of the yard may be turf.

**Rear Yards** - Rear yards shall contain at least one shade tree for every 50 linear feet of rear lot line. Trees may be placed in groups so long as the total number of trees satisfies the requirement. At least 20% of the area shall be planted in native shrubs and ground cover. The remainder of the yard may be turf.

#### G. Illustrative Photos

**Description** - The following photos illustrate the design intent of the Industrial building type.



### 3.8 Facade Types

#### 3.8.1 Commercial Facade

Commercial facades are intended to create an enhanced pedestrian-oriented environment through design elements such as high visibility into the buildings, shade fixtures, outdoor dining spaces, architectural details and other features. These commercial facade elements are governed by the following requirements:

**General**

- The facade is set back from the front property line per applicable street setback requirements typically at or near a front property line with the entrance at sidewalk grade.
- Facades may incorporate awnings, marquees and arcades that cover the sidewalk and may extend into the right-of-way, as well as galleries. (See Sec. 3.9 for details)
- Recessed entrances are acceptable.
- Building facades wider than 75 feet shall incorporate vertical divisions to mimic smaller-scale development.
- All facades facing a street or open space shall incorporate architectural elements that divide the building horizontally. Single story buildings shall contain a recognizable ground floor area and cornice area. Multi-story buildings shall contain recognizable ground floor, middle, and cornice areas.

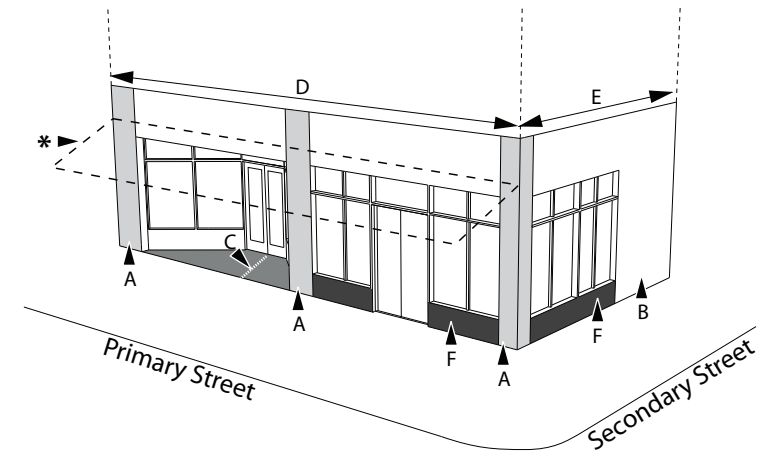
**Windows and Entrances**

- All glass shall be clear and non-reflective. If glass incorporates tinting, it should be transparent enough for those outside the buildings to see building occupants. Tinting should not be the primary strategy to provide privacy or reduce solar transmittance, but rather these should be accomplished through the articulation of buildings facades with awnings, wall thickness, canopies, marquees, arcades or galleries.

**Transparency**

- Transparency shall be calculated as the percentage (%) of clear glass between the ground and 10 feet in height.

FIGURE 3.8.1 - COMMERCIAL FACADE



\* See Section 3.9 for awning, arcade, and gallery requirements

FACADE REQUIREMENTS	MIN	MAX
A - Distance between door and/or window opening on primary facade (ft)	--	4
B - Distance between door and/or window opening on secondary facade (ft)	--	10
C - Door recess (ft)	--	5
D - Primary facade ground floor transparency (%)	60	--
E - Secondary facade ground floor transparency (%)	40	--
F - Height to bottom of window (ft)	--	2.5

### 3.8.2 Civic/Institutional Facade

Civic and institutional facades are intended to facilitate the functioning of government buildings, hospitals, schools, churches, and other public type uses, while maintaining as much of a pedestrian orientation as is feasible. It is acknowledged that such buildings are sometimes internally complex and uses are sensitive in nature. Flexibility in the requirements is warranted as follows:

#### General

- Facades may incorporate awnings, marquees, porches, and arcades.
- Building facades wider than 75 feet shall incorporate vertical divisions to mimic smaller-scale development.
- All facades facing a street or open space shall incorporate architectural elements that divide the building horizontally. Single story buildings shall contain a recognizable ground floor area and cornice area. Multi-story buildings shall contain recognizable ground floor, middle, and cornice areas.
- Buildings should have a well defined pedestrian access on the primary facade.

#### Transparency

- Transparency shall be calculated as the percentage (%) of clear glass between the ground and 10 feet in height.
- Exceptions to the transparency requirements may be granted where patient privacy or employee safety would be compromised. In such situations, the building shall be designed with features that break up the mass of the building such as treated or spandrel glass, building articulations, architectural fenestrations or green wall landscape features. For these exceptions, such details shall be counted towards the transparency requirement.

FACADE REQUIREMENTS	MIN	MAX
Primary facade ground floor transparency (%)	25	--
Secondary facade ground floor transparency (%)	25	--

### 3.8.3 Industrial Facade

Industrial facades are intended to facilitate the function of warehouse, manufacturing, or flex office/industrial uses, while maintaining a high quality visual environment. Industrial facade elements are governed by the following requirements:

#### General

- Facades may incorporate awnings, marquees and arcades.
- Building facades wider than 75 feet shall incorporate vertical divisions to mimic smaller-scale development.
- All facades facing a street or open space shall incorporate architectural elements that divide the building horizontally. Single story buildings shall contain a recognizable ground floor area and cornice area. Multi-story buildings shall contain recognizable ground floor, middle, and cornice areas.
- No exposed, unfinished sheet metal or concrete shall be permitted on building exteriors unless used as an accent to the architectural style of the building.
- No exposed, unfinished concrete block shall be permitted on building exteriors or screen walls.
- Color palette, materials, and finishes shall be consistent on all exterior elevations of the principle building.
- Any secondary buildings, accessory structures, or screen walls shall be treated consistent with the principle building.
- Gutters, downspouts, vents, louvers, and rolling doors, shall be a color consistent with the color scheme of the building they are incorporated into.

#### Transparency

- Transparency shall be calculated as the percentage (%) of clear glass between the ground and 10 feet in height.

FACADE REQUIREMENTS	MIN	MAX
Primary facade ground floor transparency (%)	10	--
Secondary facade ground floor transparency (%)	10	--

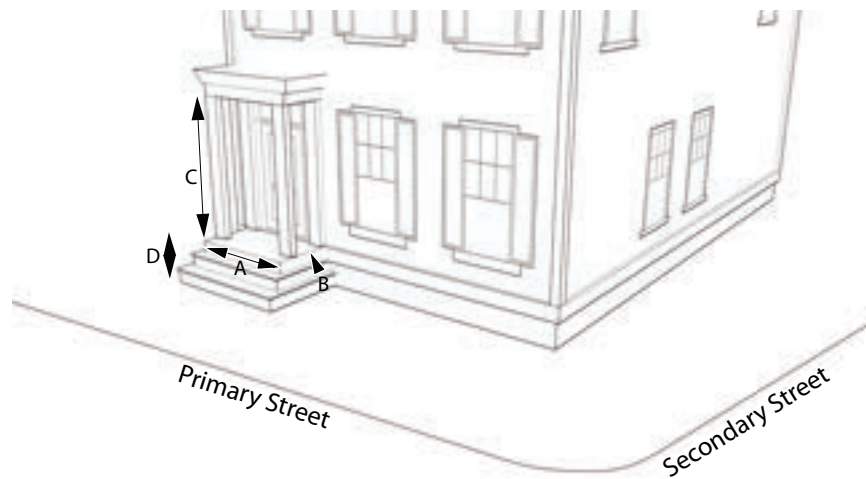
### 3.8.4 Stoop

A stoop is an elevated entrance to a building, usually of a residence, with small setbacks from the sidewalk:

**General**

- Stoops must be set back from the front lot line per applicable setback standards.
- The facade includes an elevated landing and entryway accessed by a series of steps.
- The stoop may or may not feature a roof covering the landing.
- Steps may encroach into the minimum setback but shall not encroach into the ROW.

FIGURE 3.8.4 - STOOP FACADE



Requirements	MIN	MAX
A - Width (clear)	5'	8'
B - Depth (clear)	5'	8'
C - Height (clear)	8'	--
D - Elevation	7"	3'

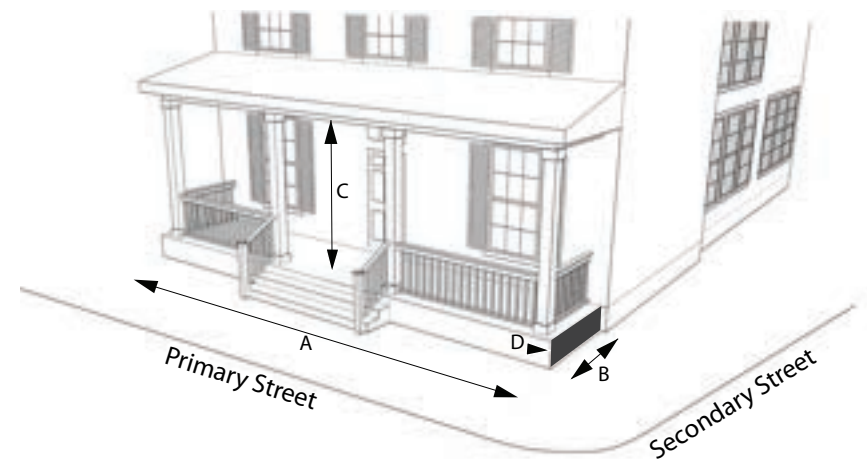
### 3.8.5 Porch

A porch is a covered entry and sitting area attached to the face of a building, at the same level as the ground floor, and accessed by a series of steps:

**General**

- Porches must be set back from the front lot line per applicable setback standards.
- Porches may wrap around the corners of the building.
- Steps may encroach into the minimum setback but shall not encroach into the ROW.

FIGURE 3.8.5 - PORCH FACADE



Requirements	MIN	MAX
A - Width (clear)	10'	--
B - Depth (clear)	5'	12'
C - Height (clear)	8'	--
D - Elevation	7"	3'

### 3.9 Encroachments

For the purpose of this Overlay, an encroachment is any building façade treatment that projects into the ROW. These treatments provide opportunities for shade, protection from the elements, outdoor dining, informal gathering places, and visual interest – all of which enhance the pedestrian environment. These special treatments are permitted according to Building Type and are encouraged to the greatest extent possible. Requirements for each encroachment type are provided below. The following types of encroachments may be permitted within the Overlay:

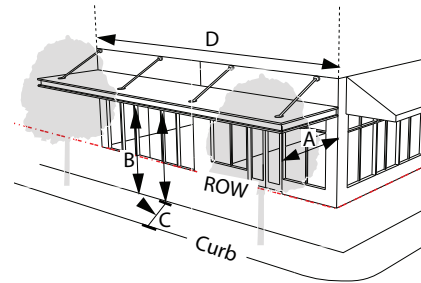
- Awnings
- Marquees
- Arcades
- Galleries
- Balconies
- Projecting Wall Signs
- Outdoor Dining

#### 3.9.1 Awnings and Marquees

**Description** - Awnings and marquees are coverings over building entrances, windows, and sidewalks that are attached to the exterior wall of the adjacent building.

- Breaks shall coincide with windows and entrance breaks.
- Awnings shall be covered with fabric, however, high-gloss or plasticized fabrics are prohibited.
- Awnings and marquees shall not encroach into the furniture/planting zone.

REQUIREMENTS	MIN	MAX
<b>A</b> - Depth	5'	--
<b>B</b> - Height (clear)	9'	--
<b>C</b> - Setback from curb	4'	--

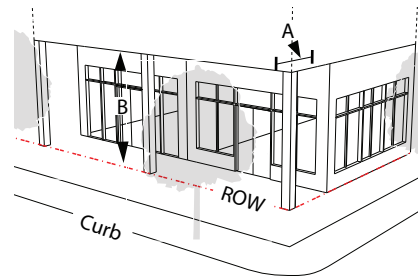


#### 3.9.2 Arcade

**Description** - An arcade is a collonade at the ground floor with usable building area on upper floors above the collonade.

- Arcades shall be supported by columns, piers or arches and shall be spaced no farther apart than they are tall.
- Enclosed usable space may be permitted above the arcade, subject to City Codes and easement requirements.
- Arcades are permitted to wrap around corners.

REQUIREMENTS	MIN	MAX
<b>A</b> - Depth	7'	--
<b>B</b> - Height (clear)	9'	--

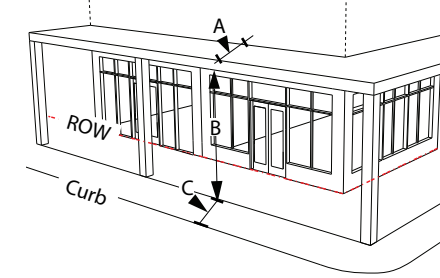


#### 3.9.3 Galleries

**Description** - A gallery is a continuous covered portion of a sidewalk supported by columns or arches.

- Gallery supports shall be positioned at by regular intervals.
- Galleries are permitted to wrap around corners.
- Galleries shall not encroach into the furniture/planting zone.

REQUIREMENTS	MIN	MAX
<b>A</b> - Depth	7'	--
<b>B</b> - Height (clear)	9'	--
<b>C</b> - Setback from curb	4'	--

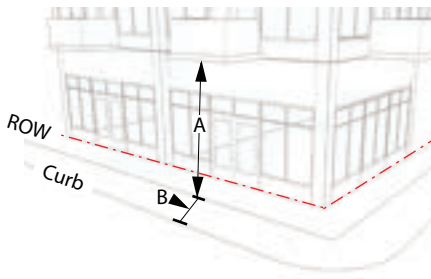


### 3.9.4 Balconies

**Description** - A platform projecting into the ROW from the upper floor of a building, which is accessible from the interior of the floor.

- Balconies shall not be supported by columns from the ground.

REQUIREMENTS	MIN	MAX
A - Height (clear)	10'	--
B - Setback from curb	4'	--

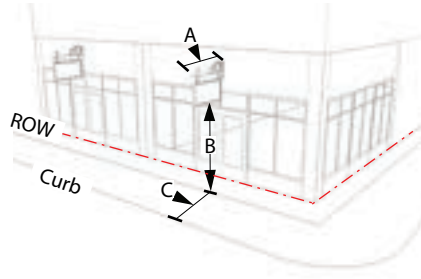


### 3.9.5 Projecting Wall Sign

**Description** - A wall-mounted sign that extends from the vertical face of a building into the ROW.

- One projecting wall sign per establishment, which must be mounted at or near the entrance of the establishment.
- They must be below the second floor window, if applicable.
- They may not be internally lit.
- Materials and style must be consistent with the architecture of the building.

REQUIREMENTS	MIN	MAX
A - Depth	--	4'
B - Height (clear)	8'	--
C - Setback from curb	2'	--

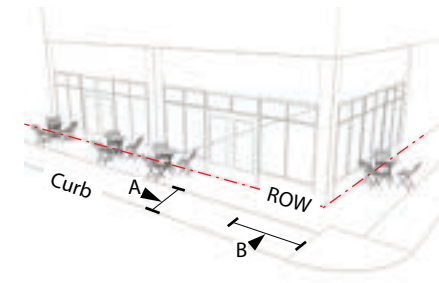


### 3.9.6 Outdoor Dining

**Description** - Outdoor seating and sidewalk cafés are permitted within the ROW immediately adjacent to the façade of a dining establishment, as shown in the illustration to the right.

- Alterations to or covering of sidewalks in outdoor seating areas are prohibited, including, but not limited to: paint, carpet, turf, platforms, etc.
- Menu boards, planters, booths and other associated furniture must be within the outdoor seating envelope.
- The outdoor seating area must be clean and free of litter, which is the responsibility of the associated establishment.
- Outdoor seating for all establishments serving alcohol must be surround by a 36-inch high free-standing, removable barrier.
- Barriers may not include chain link, chain rail, rope rail, or any material not specifically designed for fencing.
- Establishments that do not serve alcohol are not required to incorporate the barrier.
- Seating, fencing, and other outdoor furniture shall not obstruct entrances.

REQUIREMENTS	MIN	MAX
A - Clear Sidewalk	5'	--
B - Distance from corner radius	10'	--



### 3.10 Architectural Guidelines

All structures inside the Overlay should be designed consistent with the guidelines of this section. Applicants are encouraged to follow a specific style described herein. While adherence is not mandatory, consistency with this section may be evaluated with respect to granting of waivers and/or special exceptions

### 3.10.1 Main Street Commercial

The Main Street Commercial style is the architectural embodiment of Small Town America. It can be found on main streets from Florida to Maine to California, and everywhere in between. It can inspire a sense of nostalgia if applied with care, but appears obviously fake and contrived if not applied correctly. The following guidelines are provided to describe effective application of this architectural style:

#### A. Massing

- Typically rectangular boxes with flat vertical faces.
- Primary facades of single story buildings may extend slightly above the roof to create a taller front, depending on the scale of the adjacent public space.
- Subtle variations in facade height can add interest and help to break up the mass of a series of connected storefronts.
- Building corners at important intersections can also be emphasized to provide interest.

#### B. Decorative Elements

- Main Street Commercial buildings use moldings to highlight the ground floor, break up the middle stories, and provide visual interest.
- The tops of facades are typically treated with cornices or caps.
- Moldings and cornices can have a range of detail from plain to very ornate.
- Simple pilasters are permitted.
- Decorative or structural columns can separate ground floor openings and

must be a style consistent with the building as a whole.

- Stone, cast concrete, or terracotta ornaments may be used for additional accent.

#### C. Openings

- Ground floor windows and doors are large, with transoms.
- Upper floor windows group and align with the storefront/ground floor divisions below.
- Upper floor windows are usually vertically oriented and double-hung.

#### D. Roofs

- Flat
- May be accessible and used for terraces.
- Parapets shall be used to conceal any machinery on the roof visible from adjacent streets or pedestrian areas.

#### E. Materials

- **Primary Materials:** brick, painted brick, stucco
- **Accent Materials:** concrete, stone, tile, wood, cast iron
- **Prohibited Materials:** EIFS, foam moldings

#### F. Attached Elements

- Awnings, marquees, arcades, galleries, and balconies are highly encouraged to provide shelter and shade and to visually emphasize the ground floor uses.
- Projecting wall signs are encouraged.



### 3.10.2 Low Country

Low Country architecture is a uniquely southern coastal style suited for the humid subtropical climate of the region. The style is usually applied in residential construction, but it can be used for non-residential uses in certain situations such as small civic buildings. The following guidelines are provided to describe effective application of this architectural style:

#### A. Massing

- Wood framed.
- Raised on pilings so ground floor is elevated.

#### B. Decorative Elements

- Decorative elements are minimal with simple trim around windows and doors.
- Simple molding around column bases/capitals.
- Shutters are universal on ground floor windows.

#### C. Openings

- Ground floor windows under porches are large to allow heat to escape in the evening.
- Upper floor windows not covered by a porch are usually much smaller than the ground floor windows.
- Dormer windows are encouraged.
- Windows are typically double-hung.

#### D. Roofs

- Usually hip of 4/12 to 10/12 but sometimes gable, or even a combination of hip and gable.
- Eaves are typically exposed (rafter tails) but not required.
- Dormer windows usually have hip roofs, but may be shed or gabled roofs.
- Colored, raised seam metal roofs are preferred, but asphalt shingles are possible.
- Square crows nests with hip roofs are sometimes used.

#### E. Materials

- **Primary Materials:** Wood, wood siding, hardi board ok
- **Accent Materials:** Brick
- **Prohibited Materials:** EIFS, foam moldings, vinyl siding

#### F. Attached Elements

- Deep porches are universal to shield ground floor windows from the summer sun. They typically wrap around the sides of the building.
- Porch railings and support columns are simple wood.
- Porch steps are wide in proportion to the width of the porch itself.





### 3.10.3 Florida Vernacular

Like the Low Country style, the Florida Vernacular architectural style was developed in response to the humid coastal climate of central and south Florida. This style is frequently used for both residential and non-residential buildings. The following guidelines are provided to describe effective application of this architectural style:

#### A. Massing

- Asymmetrical and informal massings.
- Ground floor is not usually raised but may be depending on context.

#### B. Decorative Elements

- Decorative elements are minimal with simple trim around windows and doors.
- Columns may be square or spindle style with simple bases and capitals.
- Simple molding around column bases/ capitals.
- Simple trim (usually white) on building corners and square column corners.
- Shutters are common but not universal.
- Simple stickwork under gables is nearly universal.
- Triangle brackets are common.
- Upper floor gables usually have a different material from the ground floor, especially wood siding that runs 90° to the siding on the ground floor or half-round wood shingles.

#### C. Openings

- Double-hung and casement windows are appropriate.
- Upper floor windows not covered by a porch are usually much smaller than the ground floor windows.
- Dormer windows are encouraged.

- Upper floor windows are typically square or horizontally oriented.

#### D. Roofs

- Usually gable, but sometimes hip, or even a combination of hip and gable.
- Eaves are not usually exposed (rafter tails). Simple brackets under the eaves are more common.
- Dormer windows usually have gable roofs but may be shed roofs.
- Colored, raised seam metal roofs are preferred, but asphalt shingles are possible.
- Square crow's nests with hip roofs are sometimes used.

#### E. Materials

- **Primary Materials:** Wood siding, hardi board ok
- **Accent Materials:** stucco, wood shake/ shingle, brick, cast stone
- **Prohibited Materials:** EIFS, foam moldings, vinyl siding

#### F. Attached Elements

- Porches and awnings are common to shield doors and ground floor windows from the rain and sun.
- Porches may wrap around the sides of the building.
- Porch railings and balusters are simple wood or metal.
- Short balustrades of the same style along the tops of porches openings are common.
- Simple brackets in porch openings are common.



### 3.10.4 Craftsman

The Craftsman architectural style is a residential building style originating from the late 1800's that emphasizes handmade work and craftsmanship over mass production. It focuses on simplicity and natural materials over elaborate decoration. It is rarely used in non-residential applications. The following guidelines are provided to describe effective application of this architectural style:

#### A. Massing

- Symmetrical or asymmetrical massings are common depending on gable configurations.
- Ground floor is usually raised slightly.

#### B. Decorative Elements

- Columns may be square or tapered square.
- Column pedestals are usually brick, cast concrete, or cast stone.
- Shutters are rare.
- Upper floor gables may have a different material from the ground floor (i.e. wood siding below and wood shingles above or visa versa).
- Triangle brackets are common.
- Exposed beams, either structural or decorative.

#### C. Openings

- Double-hung windows are appropriate on the ground floor in groups of two or three side-by-side.
- Ground floor windows typically have a 2 over 1, 3 over 1, or 4 over 1 configurations, although others are possible.
- Upper floor windows are usually much smaller than the ground floor windows.
- Dormer windows are encouraged.

#### D. Roofs

- Low pitch roofs usually between 4/12 and 8/12 with deeply overhanging eaves.
- Usually gable, but sometimes hip, or even a combination of hip and gable.
- Gable configurations include: Side Gable, Front Gable, Double Front Gable, and Cross Gable.
- Eaves are usually exposed (rafter tails).
- Dormer windows usually have gable roofs but may be shed or cut-off gable.
- Asphalt shingles are preferred.

#### E. Materials

- **Primary Materials:** Wood siding, hardi board ok, wood shake/shingle (coursed or staggered)
- **Accent Materials:** stucco, wood shake/shingle, brick, cast stone
- **Prohibited Materials:** EIFS, foam moldings, vinyl siding

#### F. Attached Elements

- Porches and stoops are extensions of the roof or gabled end.
- Porch and stoop railings are either simple wood or metal balustrades or are solid wood or concrete.
- Simple triangular brackets under gabled porch eaves are common.



### 3.10.5 Modern/Contemporary

Modern architecture is a wide ranging style composed of many different branches and interpretations that can make it difficult to describe. The intent here is for “Modern” or “Contemporary” style to refer to an architectural style featuring basic geometric forms that create large volumes, long lines and smooth walls. The following guidelines are provided to describe effective application of this architectural style:

#### A. Massing

- Series of geometric shapes that form large volumes.
- Symmetrical or asymmetrical massings.
- Building entrances should have a distinguishing element or massing.
- Must balance massing with the need for a pleasant pedestrian environment at the ground floor.

#### B. Decorative Elements

- Sparse decorations and the materials of the building are allowed to speak for themselves.
- Structural members are exposed and celebrated.

#### C. Openings

- Large glass curtain walls are nearly universal.
- Doors are usually glass.

#### D. Roofs

- Typically flat, but sometimes shed.
- Eaves and overhangs are uncommon but can exist in the right context, especially when applying a modern twist on traditional styles such as Florida Vernacular or Craftsman.

#### E. Materials

- **Primary Materials:** Glass, finished concrete, metal
- **Accent Materials:** Metal, exposed concrete, brick, wood, stucco
- **Prohibited Materials:** EIFS, foam moldings

#### F. Attached Elements

- Marquees are common.
- Porches and stoops are not common but could be incorporated if they fit with the overall architecture of the building.



### 3.11 Street Standards

The following standards are for all public streetscapes within the district, whether they are associated with roads on City rights-of-way or on roads that are platted but un-dedicated. Specific street sections are provided in Sections 3.11.5.

#### 3.11.1 Streetscape Zones

Streetscapes are divided into several zones based on their functions. These include:

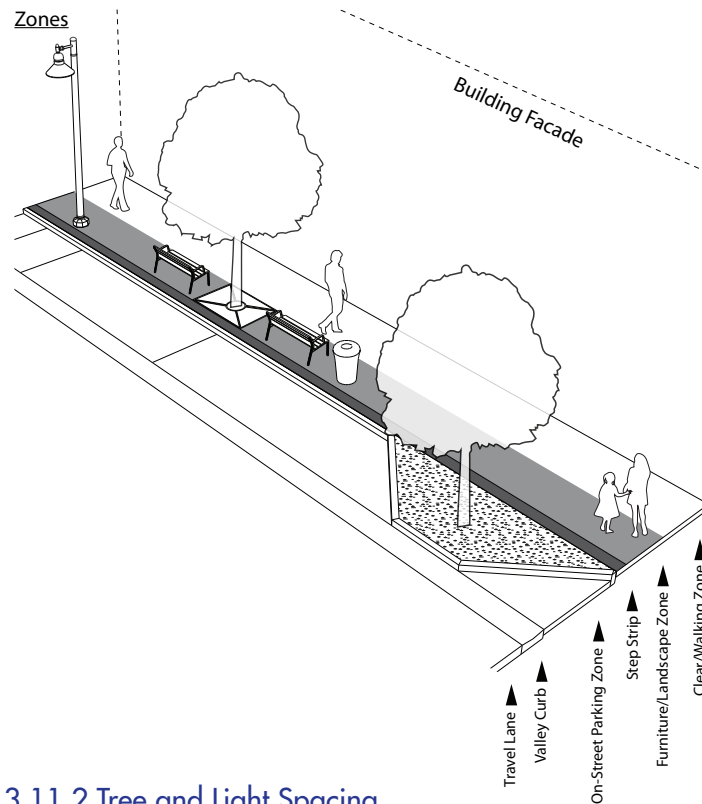
**Travelway** – This zone includes travel lanes for cars and bicycles as well as turn lanes. Travel lanes in downtown range from 10-11 feet in width. Bike lanes are between 4 and 6.5 feet.

**On-Street Parking Zone** – This zone includes parallel parking for vehicles with a 1.5 to 2-foot valley gutter between the parking and the travel lanes. On-street parking bays are between 6.5 and 7 feet wide with bulb-outs every 3-4 spaces.

**Furniture/Landscape Zone** – Where on-street parking does not exist, this zone may be a planted landscape strip. When adjacent to on-street parking, this zone must function as a hardscape furniture zone. A 1-foot step strip is preferred between street trees/planters and on-street parking.

**Clear/Walking Zone** – This zone is open sidewalk, unobstructed by trees, lights, or other street furniture. The clear zone shall have a minimum width of 5 feet.

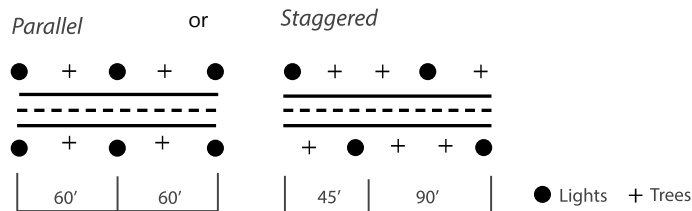
FIGURE 3.11.1 - STREETSCAPE ZONE DIAGRAM



#### 3.11.2 Tree and Light Spacing

Typical street tree and light pole spacing shall be as follows, subject to the specific conditions of the street:

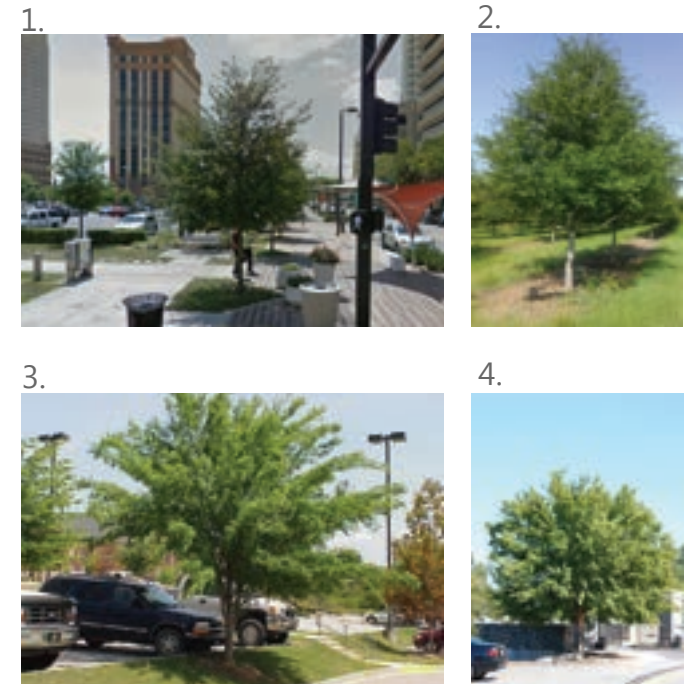
FIGURE 3.11.2 - STREETSCAPE ZONE DIAGRAM



#### 3.11.3 Street Trees

Street trees approved for use within the Overlay:

1. Sky Climber Live Oak (*Quercus virginiana* 'Sky Climber')
2. Cathedral Live Oak (*Quercus virginiana* 'Cathedral')
3. Bosque Elm (*Ulmus parvifolia* 'Bosque')
4. Allee Elm (*Ulmus parvifolia* 'Allee')



- Sky Climber is the preferred street tree, but may not always be available. Cathedral is an appropriate substitute in those cases.
- Allee or Bosque Elm trees may be used where space or height limitations make oaks unsuitable.

### 3.11.4 Stormwater Planters and Low Impact Development

Stormwater is a major limiting factor in the redevelopment of downtown. Though a master stormwater system for the downtown area will help, it will not be sufficient to accommodate the entire downtown or all potential development. There must be a focus on the upstream treatment train on both the public and private sides to reserve as much capacity for the master system as possible. Low Impact Development (LID) strategies will be critical to accomplishing this goal.

#### General

- Stormwater planters are encouraged in lieu of raised or at grade tree lawns or landscaped bulb-outs where technically feasible and innocuous to the functional movement of vehicles and pedestrians.
- Other LID strategies are encouraged for use in parking lots and other development areas, where appropriate.

#### Planter Trees

Trees approved for use in stormwater planters are:

1. Sabal Palm (*Sabal palmetto*)
2. Bald Cypress (*Taxodium distichum*)

1.



2.



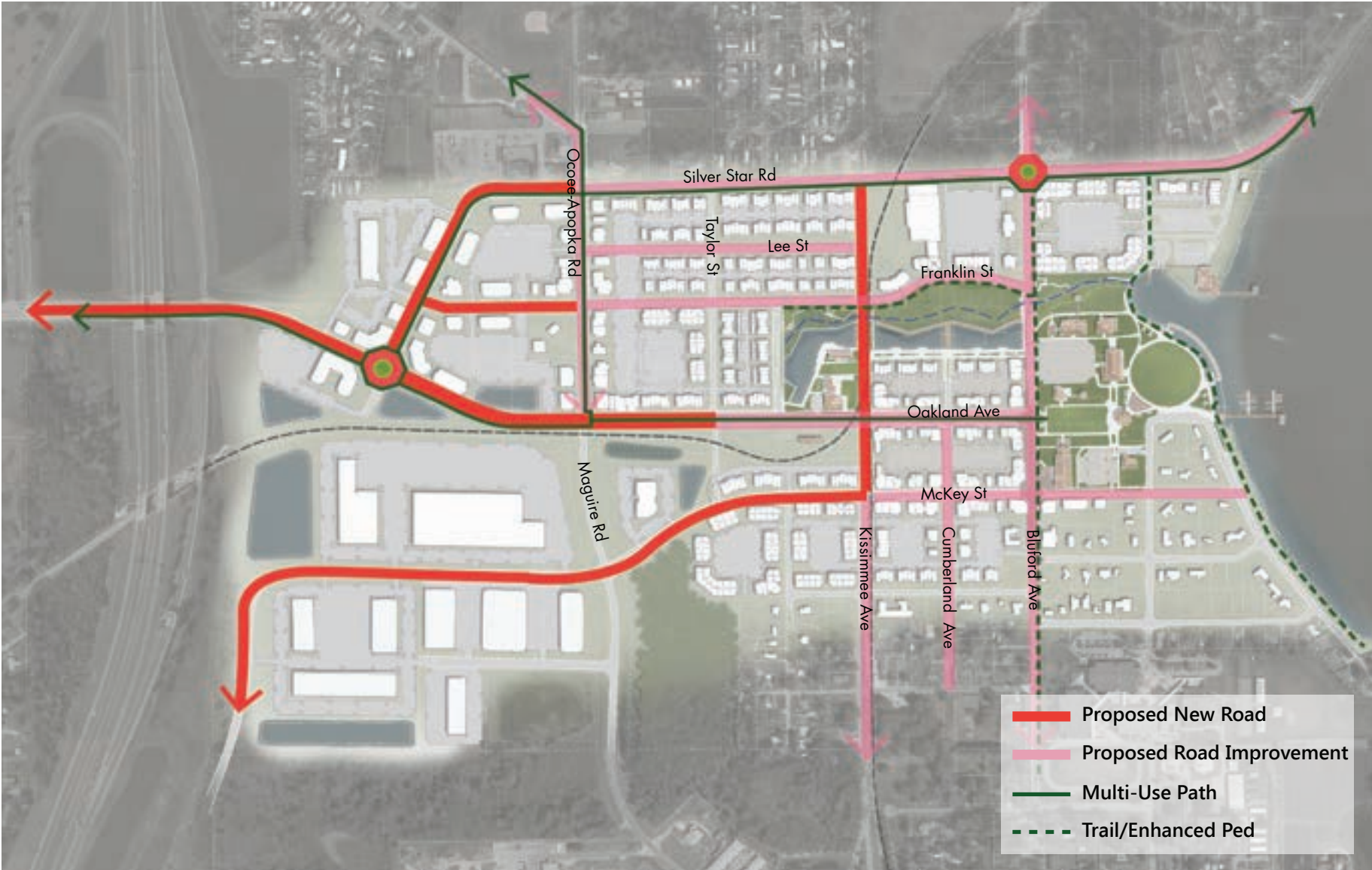
Trees may be used in stormwater planters where sufficient room is available and intersection sight lines will not be impeded. Sabal palms may be used in stormwater planters where visibility and site lines are critical, such as at intersections. Bald cypress may be used where site lines are not an issue.



### 3.11.5 Typical Street Sections

The following streets sections are typical examples of streets within the Overlay. Changes to the typical sections may be made on a case-by-case basis to accommodate different conditions that may arise throughout the Overlay area.

# Figure 3.11.5 Street Section Map



## W SILVER STAR RD: Franklin St to Lakeshore Dr

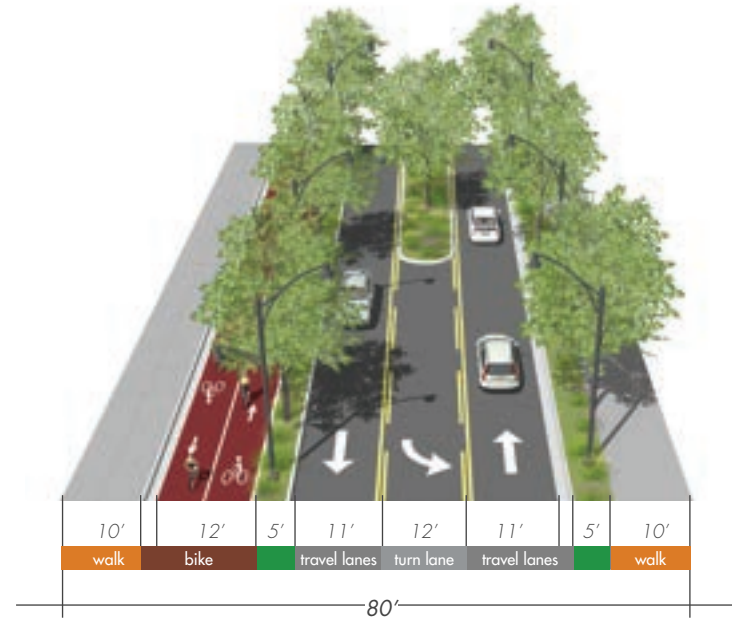
Existing Conditions: 80' Right-of-Way



Silver Star Road (SR 437) currently has a two-lane rural section with no curbs, open drainage, and intermittent sidewalks within an 80 foot ROW. Intersections with Ocoee-Apopka Rd and Bluford Ave have left turn lanes.

The MPO has consistently placed Silver Star in the 5th year of the 5-year plan for a PD&E study for expansion, but has not moved forward to date. It is possible that the study may require Silver Star to be four-laned through downtown. Though this is not Ocoee's preferred option, the City would like to drive the design of the road to compliment the vision for downtown to the greatest extent possible.

Proposed Conditions: 80' Right-of-Way



Road Width	80'
Pavement Width	34'
Median	12'
Traffic Lanes	11'
Parking	none
Curb Type	vertical
Sidewalk Width	10'
Planter Width	5'
Bike	12' protected lane

The sections on this page represent Ocoee's preferred options for the two legs of Silver Star Road through downtown. From Franklin Street to Lakeshore Drive, a two lane with center turn lane is preferred, while a two lane section is still preferred east of Lakeshore Drive. Both of these options can be accommodated within the existing 80' ROW.

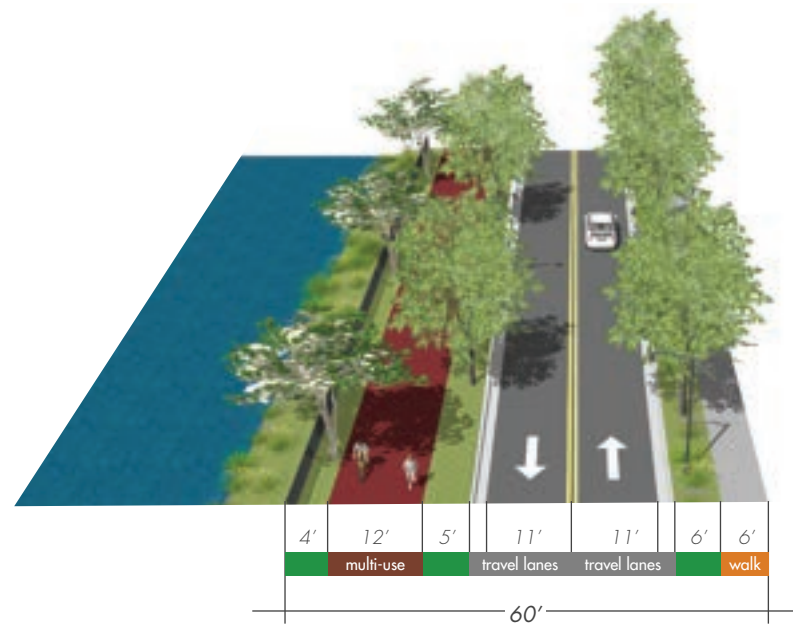
## E SILVER STAR RD: Lakeshore Dr to Spring Street

Existing Conditions: 80' Right-of-Way



East of Lakeshore Drive, Silver Star Road maintains its 80' ROW but has no sidewalks. The ROW is somewhat constrained by slopes to the north and the lake edge to the south. Expansion of the road would be difficult and might require re-profiling of the bank.

Proposed Conditions: 60' Right-of-Way



Road Width	60'
Pavement Width	22'
Median/Turn Lane	N/A
Traffic Lanes	11'
Parking	none
Curb Type	vertical
Sidewalk Width	6'
Planter Width	4'-6'
Multi-Use Trail	12' path



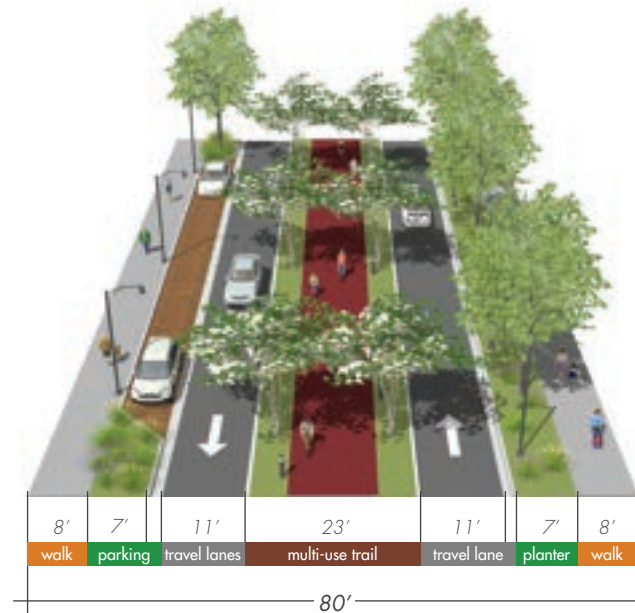
## OAKLAND AVE: Bluford Ave to Maguire Rd

Existing Conditions: 80' Right-of-Way



Oakland Ave currently has only 20 feet of pavement, some curbs, and some sidewalks within its 80 foot ROW. The future section of Oakland will be significantly different with a central median and multi-use trail leading to the lake. On-street parking is an option on one or both sides of the street, where appropriate. Eight-foot sidewalks are possible with this section, but if wider sidewalks are desired, then easements will be required from adjacent properties as they redevelop.

Proposed Conditions: 80' Right-of-Way (typical)

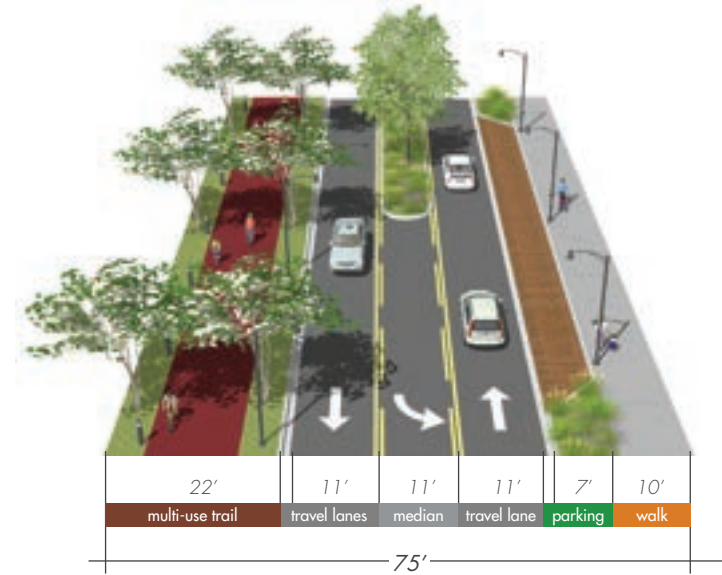


Road Width	80'
Pavement Width	22'
Median/Multi-Use Trail	23'
Traffic Lanes	11'
Parking	parallel
Curb Type	vertical
Sidewalk Width	8'
Planter Width	7'
Bike	12' path

## OAKLAND AVE: Maguire Rd to Silver Star Rd

For the extension of Oakland west of Ocoee-Apopka, the trail comes out of the central median and onto the south side of the road. This allows the ROW to shrink to 75'. On-street parking is possible on the north side of the road.

### Proposed Conditions: 75' Right-of-Way



Road Width	75'
Pavement Width	33'
Median/Turn Lane	11'
Traffic Lanes	11'
Parking	parallel
Curb Type	vertical
Sidewalk Width	10'
Planter Width	7'
Multi-use Trail	22', 12' path

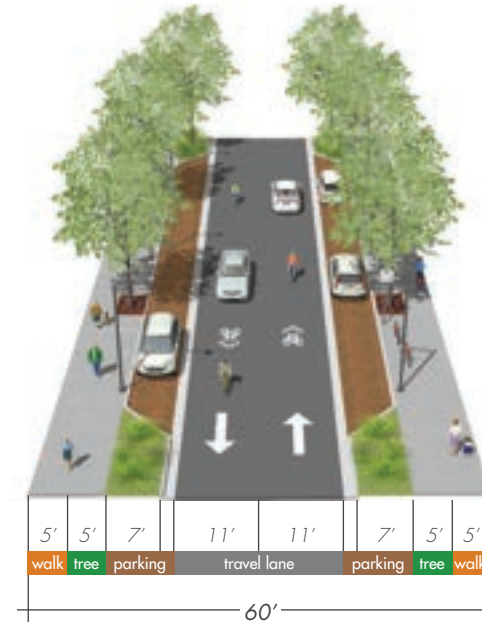
## MCKEY ST: Kissimmee Rd to Maguire Rd

Existing Conditions: 60' Right-of-Way



McKey Street, the historical main street of Ocoee has an existing ROW of 60' east of Kissimmee Avenue and approximately 50' ROW west of Kissimmee Avenue. From Bluford Avenue to Kissimmee Avenue, the street was previously improved with on-street parking (some head in angled and some parallel), planters, and traditional furniture. The future condition will extend the special treatment of McKey Street east to Lakewood Avenue and west to Maguire Road.

Proposed Conditions: 60' Right-of-Way

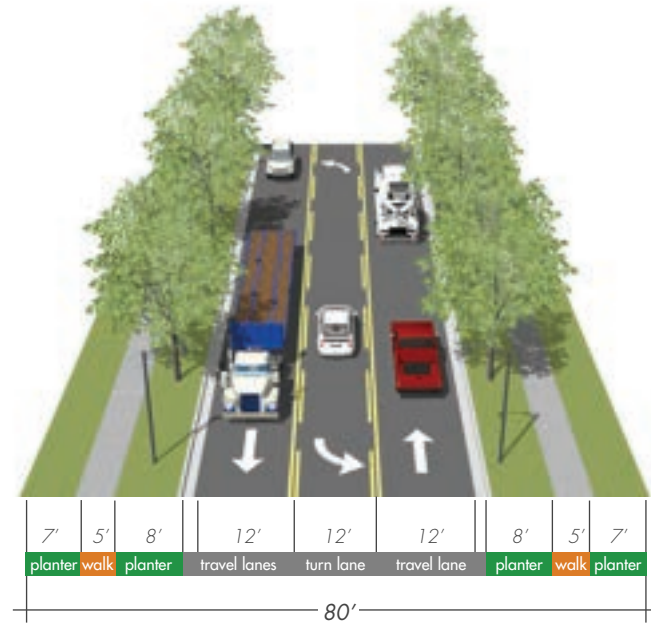


Road Width	60'
Pavement Width	22'
Median	none
Traffic Lanes	11'
Parking	parallel
Curb Type	vertical
Sidewalk Width	5'
Planter Width	5'
Bike	sharrow

## MCKEY ST: Maguire Rd to Enterprise St

West of Maguire Road, the extension of McKey Street will pass through a future industrial/office development area and the section is tailored to accommodate those types of uses. The preferred ROW is 80' to accommodate wide travel lanes, a turn lane, and sufficient buffer. There is currently no existing ROW for this segment.

### Proposed Conditions: 80' Right-of-Way



Road Width	80'
Pavement Width	36'
Median/Turn Lane	12'
Traffic Lanes	12'
Parking	none
Curb Type	vertical
Sidewalk Width	5'
Planter Width	8'
Bike	none

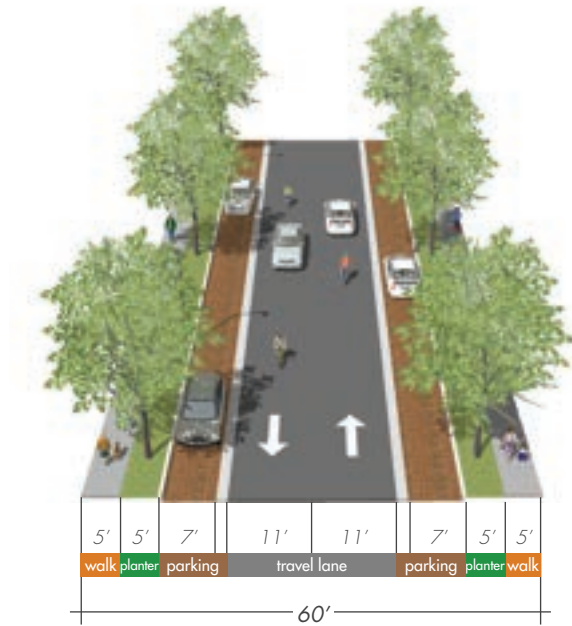
## FRANKLIN ST: Silver Star Rd to Bluford Ave

Existing Conditions: *Right-of-Way Varies*



Franklin Street varies between a 40' ROW east Kissimmee Avenue to a 60' ROW west of Kissimmee Avenue, with a sidewalk on the north side, and no curbs. Some structures are very close or even extending into the ROW. The future section will utilize the full ROW including on-street parking and sidewalks. The segment with the 40' ROW will be expanded to 60' during the master stormwater system project.

Proposed Conditions: *60' Right-of-Way*



Road Width	60'
Pavement Width	22'
Median/Turn Lane	none
Traffic Lanes	11'
Parking	parallel
Curb Type	vertical
Sidewalk Width	5'
Planter Width	5'
Bike	none

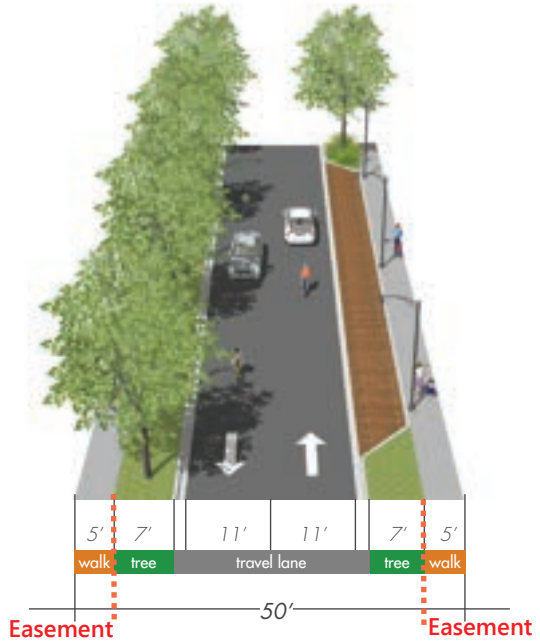
## LEE ST: Kissimmee Ave to Maguire Rd

Existing Conditions: 40' Right-of-Way



Lee Street is a rural lane with a 40' ROW, 15' to 18' of pavement, no curbs, and no sidewalks. The preferred future section for this road will bring it up to a full two lanes with sidewalks and optional on-street parking. The 50' that this section requires means 5' easements or additional ROW will be needed on each side.

Proposed Conditions: 40' Right-of-Way + 10' Sidewalk Easement



Road Width	50'
Pavement Width	22'
Median	none
Traffic Lanes	11'
Parking	parallel
Curb Type	vertical
Sidewalk Width	5'
Planter Width	7'
Bike	none

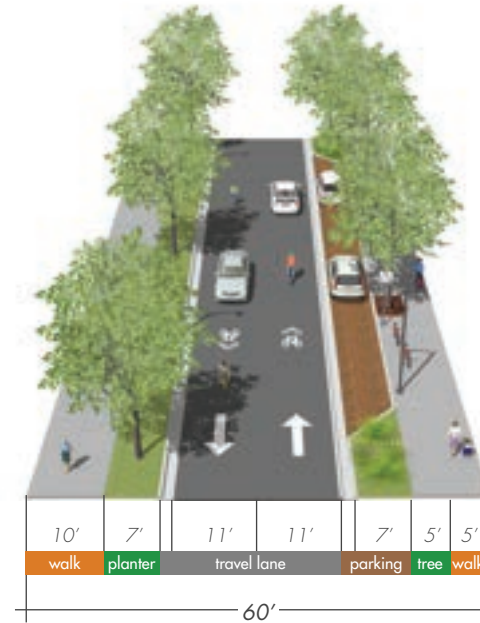
## BLUFORD AVE: Typical Section

Existing Conditions: *Right-of-Way Varies*



Bluford Avenue has a variety of ROW widths from 80' to 53' and equally varied adjacent conditions, so a typical section has been provided here. Options range from an urban section with tree grates and on-street parking to a more rural section with tree lawns. Wide sidewalks are provided to allow for a safer path to school from the north.

Proposed Conditions: *60' Right-of-Way*



Road Width	varies
Pavement Width	22'
Median	none
Traffic Lanes	11'
Parking	parallel
Curb Type	vertical
Sidewalk Width	5'-10'
Planter Width	varies
Bike	sharrow

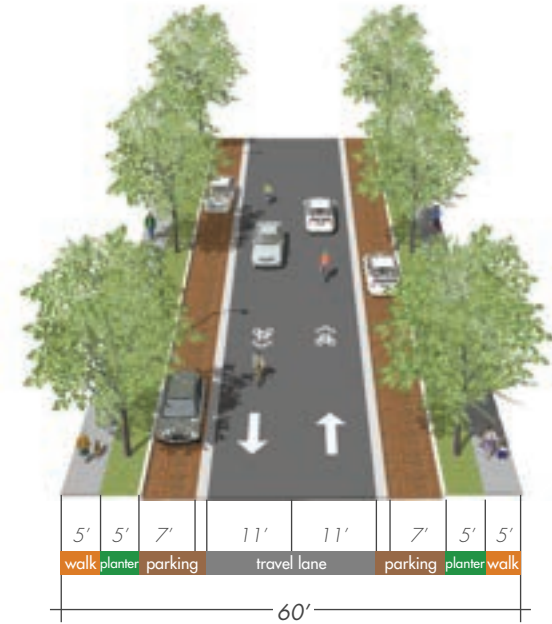
# CUMBERLAND AVE From Floral St to Franklin St

Existing Conditions: 60' Right-of-Way



Cumberland Avenue is a 60' ROW with 20' of pavement, valley gutters, no curbs, and a sidewalk on the west side. In the blocks between McKey Street and Oakland Avenue, Cumberland is more urban with curbs and some on-street parking. The proposed section formalizes a more urban section for its entire length, with on-street parking lanes, tree lawns, and sidewalks on both sides.

Proposed Conditions: 60' Right-of-Way



Road Width	60'
Pavement Width	39'
Median	none
Traffic Lanes	11'
Parking	parallel
Curb Type	vertical
Sidewalk Width	5'
Planter Width	5'
Bike	sharrow



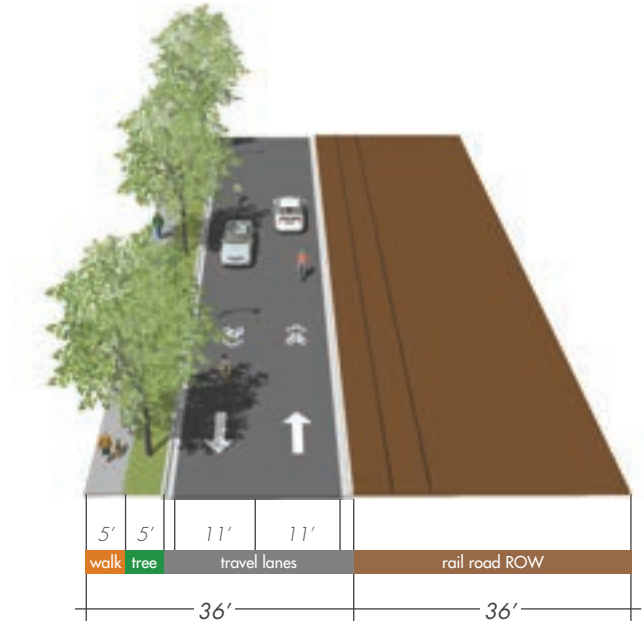
## KISSIMMEE AVE: Silver Star Rd and McKey St

Existing Conditions: *Right-of-Way Varies*



Kissimmee Avenue is a mix of existing road, un-paved lane, and missing segments, all constrained on the east side by railroad tracks. The current ROWs range from 40' south of Floral Street to approximately 20' north of Floral Street. Missing segments include from McKey Street to Franklin Street and from Lee Street Silver Star Road. The end goal is for Kissimmee Avenue to connect through from McKey Street to Silver Star Road with a common section. The section proposed will require additional ROW in certain segments.

Proposed Conditions: *36' Right-of-Way*



Road Width	36'
Pavement Width	22'
Median/Trail	none
Traffic Lanes	11'
Parking	none
Curb Type	vertical
Sidewalk Width	5'
Planter Width	5'
Bike	sharrow

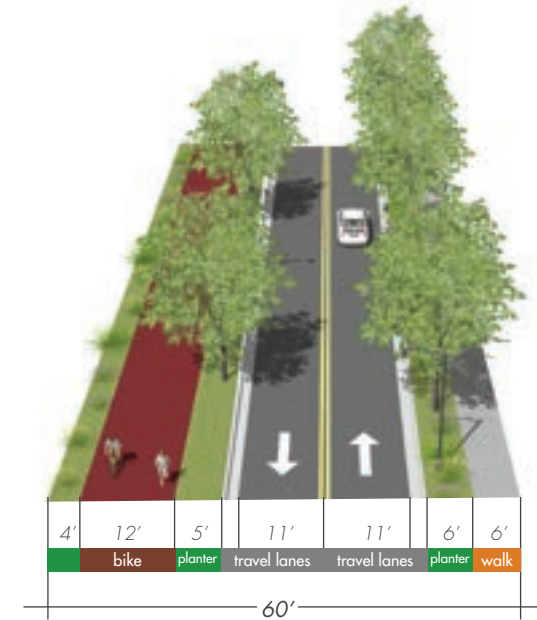
## TAYLOR ST: Franklin St to McKey St

Proposed Conditions: 60' Right-of-Way



In the Long Term Vision Plan, the Taylor Street diagonal is shown to be removed and squared up to create regular development blocks. This is potentially a long-term play and will not happen for an extended period. The downtown cannot wait for Taylor Street to be realigned to have a proper entrance into downtown and a connection to the WOT. In the mean-time, Taylor Street will be reconstructed with curbs, sidewalk, and multi-use path that will connect with the WOT via Franklin/Plant.

Proposed Conditions: 60' Right-of-Way



Road Width	60'
Pavement Width	22'
Median	none
Traffic Lanes	11'
Parking	none
Curb Type	Vertical
Sidewalk Width	6'
Planter Width	4'-6'
Bike	12' path





**COMMUNITY  
SOLUTIONS  
GROUP**

618 E South Street  
Suite 700  
Orlando, FL 32801  
T 407.423.8398  
F 407.843.1070

*A GAI Consultants, Inc. Service Group*